SITE DEVELOPMENT PLAN

Reviewed for code compliance Joshua Ooyman 08/28/2024 1:33:38 PM

M Holden 08/28/2024 4:19:55 PM

INDEX OF PLANS

SHEET NO. DESCRIPTION **COVER SHEET** EROSION CONTROL / AERIAL SITE DEVELOPMENT PLAN WATER AND SEWER PLAN PAVING GRADING AND DRAINAGE DETAIL& SECTION SHEET DETAIL SHEET FIRE PLAN

PROJECT DATA

PROJECT NAME: Marco Luxe

PROJECT NO.:

ZONING:

FOLIO: 57190640006, 57190680008,

57190720007, 57190760009

FEMA FL. ZONE: AE 10.0' (2024 MAP)

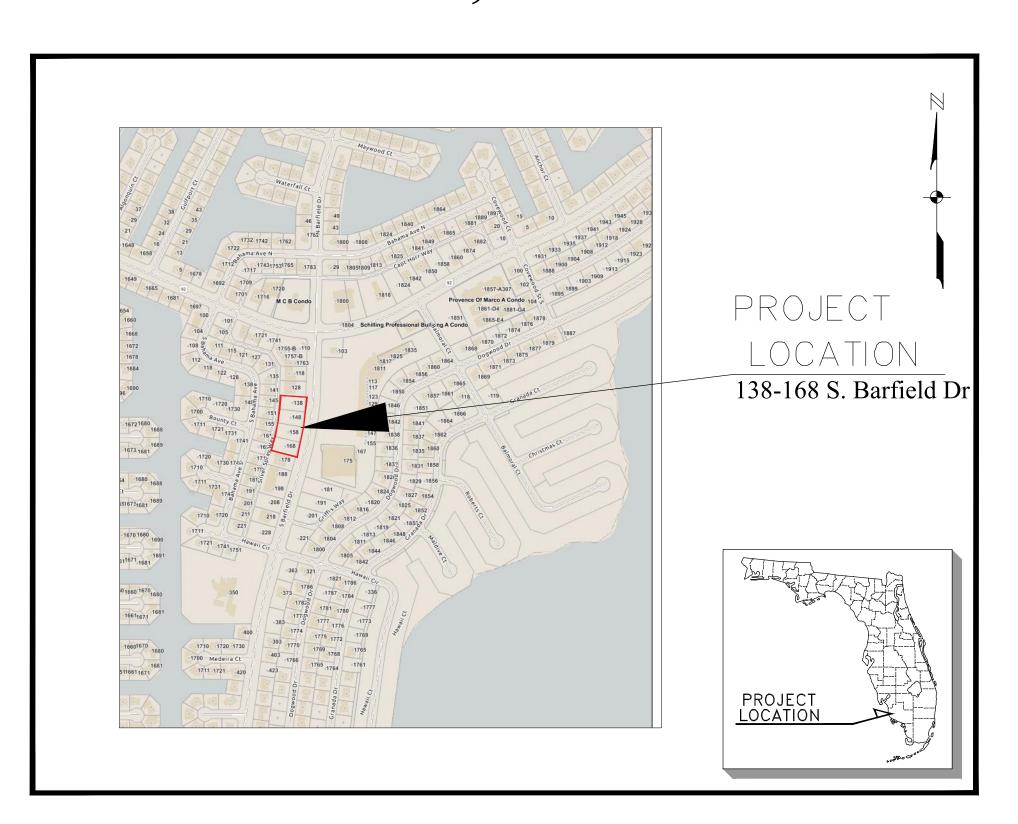
BUILDING DATA

BUILDING CODE REFERENCES: FLORIDA FIRE PREVENTION CODE **8TH ED.** 2023, 8TH ED. FLORIDA BUILDING CODE BUILDING CODE DATA: **OCCUPANCY:** STORAGE (S-1) **CONSTRUCTION TYPE:** FBC TYPE IIB, SPRINKLED **BUILDING HEIGHT:** MAX HEIGHT: 50' HEIGHT (C-3) PROPOSED HEIGHT: 2 STORIES, 23'-6" **BUILDING FLOOR AREA SUMMARY:** PROPOSED: **PROPOSED** BUILDING 1 (AREA UNDER A/C) 6,909 SF BUILDING 2 (AREA UNDER A/C) 13,991 SF 6,909 SF BUILDING 3 (AREA UNDER A/C) TOTAL (AREA UNDER A/C) 27,809 SF 55 SF BUILDING 1 COVERED ENTRY 110 SF BUILDING 2 COVERED ENTRY 55 SF BUILDING 3 COVERED ENTRY

Marco Luxe

138-168 S. BARFIELD DR. MARCO ISLAND, FL 34145 S9/ T52S/ R26E

Prepared for Owner: Marco Luxe LLC 1083 N Collier Blvd., #340 Marco Island, FL 34145





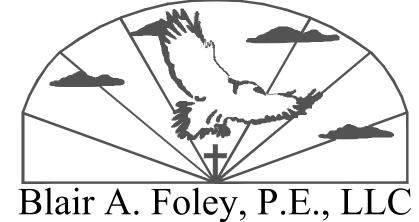
LEGAL DESCRIPTION

MARCO BCH UNIT 5 BLK 83 LOT S 7-10

DRAWING COORDINATION

ARCHITECT:

SheelyArchitects Inc 8200 College Parkway Ft. Myers, FL 33919 (239) 482-2121



Civil Engineer / Development Consultant 120 Edgemere Way South - Naples, FL 34105 Phone (239) 263-1222 - Cell (239) 289-4900 - Fax (239) 263-0472 - E-mail fols000@aol.com

Blair A Foley Foley Date: 2024.07.09 13:35:42

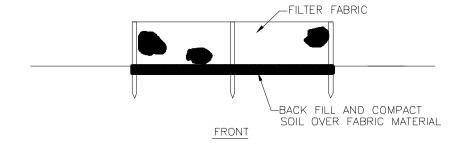
Digitally signed by Blair A

Certificate Of Authorization No. 9469 Florida Registered Professional Engineer No. 53756

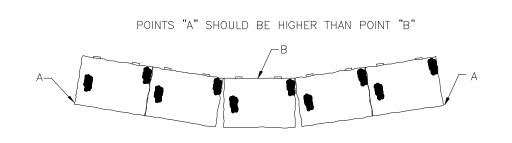
MARCO LUXE



<u>FLUCCS CODES:</u> 190-DISTURBED/MOWED GRASS

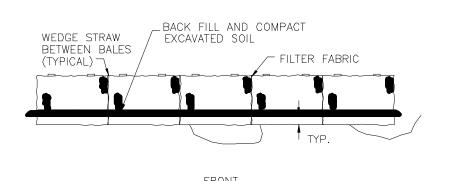


FILTER BARRIER

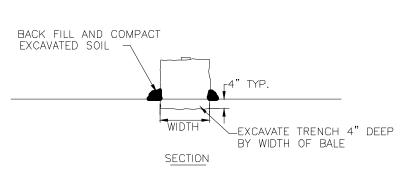


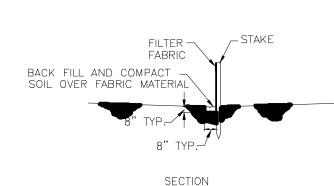
IN DRAINAGE WAY

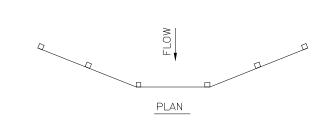
NOT TO SCALE



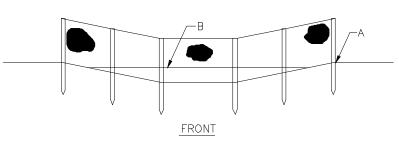
TRAW BALE BARRIE







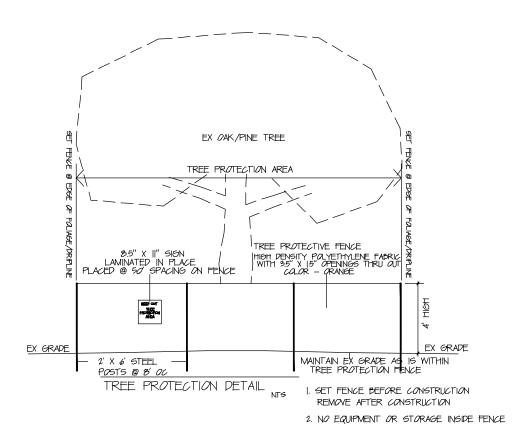
POINTS "A" SHOULD BE HIGHER THAN POINT "B"

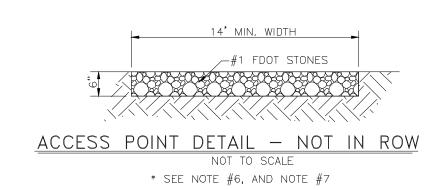


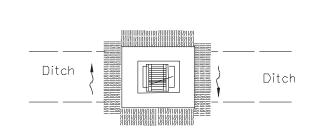
PROPER PLACEMENT OF FILTER BARRIER

IN DRAINAGE WAY

NOT TO SCALE







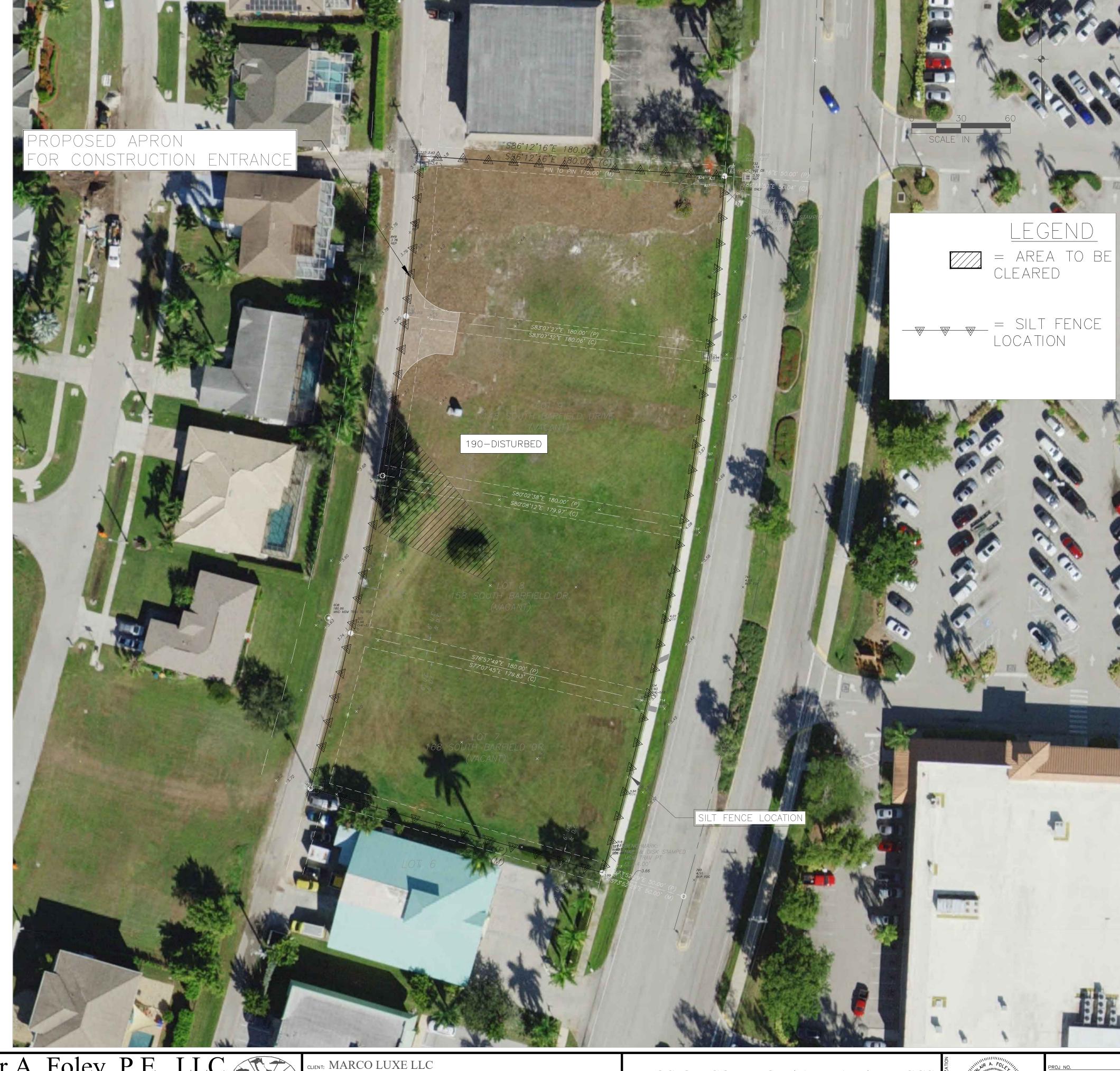
PROTECTION AROUND INLETS
OR SIMILAR STRUCTURES

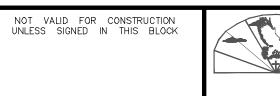
CLEARING AND EROSION NOTES:

- 1. EXOTIC VEGETATION, AS DEFINED BY THE COLLIER COUNTY LAND DEVELOPMENT CODE (LDC), SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER.
- 2. THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZE THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EASEMENTS OF RECORD.
- 3. THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM.
- 4. BARRICADES AND SILT BARRIERS WILL REMAIN IN PLACE UNTIL FINAL GRADING AND SEEDING OR SODDING IS COMPLETE.
- 5. ALL PROPOSED INLETS SHALL BE PROTECTED WITH FILTER FABRIC DURING CONSTRUCTION.
- ACCESS POINT SHALL BE A MINIMUM 6" THICK #1 FDOT STONE, AND IS TO EXTEND A MINIMUM OF 50' INTO THE PROJECT.
 NO GRAVEL OR STONE SHALL BE WITHIN ANY ROW. A CONCRETE APRON SHALL BE PROVIDED FROM THE ASPHALT TO THE ROW LINE.
 USE BARRICADES TO PROTECT VEGETATION TO REMAIN. SEE DETAIL

BOUNDARY & EXISTING CONDITIONS FROM: JOHNSON ENGINEERING, INC

AREA TO BE CLEARED = 0.1 ACRE





Blair A. Foley, P.E., LLC

Civil Engineer / Development Consultant

120 Edgemere Way South - Naples, Fl. 34105
Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols000@aol.com

MARO LUXE LLC
MARO LUXE

Collier County, Florida

EROSION CONTROL / AERIAL / FLUCCS
CLEARING PLAN



PROJ NO.

SCALE 1" = 30'

DATE 4/9/2024SHT 2 OF 8

GENERAL NOTES

1. ELEVATIONS ARE REFERENCED TO NAVD OF 1988.

FORTY EIGHT (48) HOURS PRIOR TO THE REQUESTED INSPECTION.

- 2. CURRENT ZONING FOR THIS PARCEL IS C-3.
- 3. THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZED THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EXISTING EASEMENTS OF RECORD.
- 4. HANDICAP PARKING AND ACCESSIBILITY SHALL BE IN ACCORDANCE WITH THE LATEST STATE OF FLORIDA REQUIREMENTS OR THE AMERICANS WITH DISABILITY ACT (ADA) WHICHEVER IS MORE STRINGENT.

 5. ALL DRIVEWAYS, LANDSCAPING, SIGNS, GRASS, ETC. SHALL BE RESTORED TO A CONDITION
- EQUIVALENT TO PRE CONSTRUCTION CONDITION UNLESS OTHERWISE APPROVED BY THE ENGINEER AND COUNTY.

 6. REQUESTS FOR INSPECTIONS SHALL BE PROVIDED TO CITY OF MARCO ISLAND STAFF AT LEAST
- 7. THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE COUNTY AND ALL UTILITY COMPANIES IN THE AREA 48 HOURS MINIMUM PRIOR TO START OF CONSTRUCTION, AND SHALL HAVE ALL EXISTING UTILITY LINES (SPRINT, FPL, WATER, CABLE, SANITARY, SEWER, IRRIGATION, FORCE MAIN & ANY OTHERS) LOCATED AND FLAGGED PRIOR TO ANY EXCAVATION.
- 8. CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ENGINEER PRIOR TO CONSTRUCTION.
- 9. THE CONTRACTOR IS REQUIRED TO OBTAIN WRITTEN APPROVAL FROM THE ENGINEER FOR ANY DEVIATIONS FROM THE APPROVED PLANS AND/OR SPECIFICATIONS.
- 10. CONTRACTOR SHALL PROMPTLY REPORT ALL FIELD CHANGES TO THE ENGINEER.
- 11. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS, ALL EXISTING CONFLICTS TO DESIGN PLANS (I.E. POLES, BOXES, VALVES ETC.), AND ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY REQUIRED PLAN DEVIATIONS. CONTRACTOR IS SOLELY RESPONSIBLE FOR ALL DAMAGES AS A RESULT OF ALL CONFLICTS NOT SATISFACTORLY RESOLVED. ENGINEER IS NOT LIABLE FOR ANY CONFLICT SHOWN OR OMMITTED FROM THIS PLAN OR SURVEY.
- 12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC AND USAGE OF THE EXISTING STREETS ADJACENT TO THE PROJECT SITE. ALL TRAFFIC MAINTENANCE CONTROL SHALL BE IN ACCORDANCE WITH THE FLORIDA MANUAL OF TRAFFIC AND SAFE PRACTICES FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, AND UTILITY OPERATIONS.
- 13. ALL DISTURBED PERVIOUS AREAS NOT LANDSCAPED SHALL BE SODDED AND/OR SEEDED AND MULCHED AS DIRECTED BY THE OWNER OR ENGINEER.
- DIMENSIONS WHICH REFERENCE CURBING ARE SHOWN TO FACE OF CURB UNLESS OTHERWISE NOTED.
 ALL CONSTRUCTION METHODS AND MATERIALS TO CITY OF MARCO ISLAND LAND DEVELOPMENT CODE AND F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION.
- 16. SEE CLEARING PLAN FOR LIMITS OF CLEARING.
- 17. THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE INFRASTRUCTURE, OPEN SPACES, & PAVED AREAS ON THEIR PROPERTY.
- 18. FINAL RESTORATION UPON REMOVAL OF TEMPORARY CONSTRUCTION ENTRANCE: UPON COMPLETION OF THE PROJECT, THE TEMPORARY CONSTRUCTION ENTRANCE AND ASSOCIATED IMPROVEMENTS SHALL BE REMOVED AND THE RIGHT—OF—WAY, INCLUDING GROUND COVER SHALL BE RESTORED

<u>LANDSCAPINO</u>

- 1. ALL EXOTIC VEGETATION AS DEFINED BY THE CITY OF MARCO ISLAND L.D.C. SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE RESPONSIBILITY OF THE OWNER.
- 2. THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR THE MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM.

 3. CONTRACTOR SHALL REMOVE LIMEROCK WITHIN LANDSCAPE ISLANDS LOCATED WITHIN AND ADJACENT TO THE PARKING AREAS. THESE AREAS SHALL FILLED WITH CLEAN DIRT OR AS DIRECTED BY THE LANDSCAPE ARCHITECT.

PAVING, GRADING AND DRAINAGE NOTES:

- 1. THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE SURFACE WATER MANAGEMENT SYSTEM AS OUTLINED BY THE DESIGN ENGINEER ON THESE DRAWINGS
- 2. ALL ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM (NAVD) 1988.
 3. MIN. FINISHED FLOOR ELEVATIONS SHALL BE AS SHOWN ON THE SITE PLAN.
- 4. SITE IS LOCATED WITHIN THE FLOOD ZONE AE.
 5. HAY BALES, SILT SCREENS AND OTHER EROSION CONTROL MEASURES SHALL BE UTILIZED TO
- CONTAIN ALL DISTURBED AREAS IN ACCORDANCE WITH THE EROSION CONTROL DETAILS.

 6. SILT SCREENS TO BE INSTALLED AROUND PERIMETER OF PROPERTY AND/OR DISTURBED AREAS AND MAINTAINED DURING CONSTRUCTION.
- 7. ALL PROPOSED ON—SITE PAVING SHALL BE ASPHALT, UNLESS OTHERWISE NOTED.
- 8. NOTE ALL BLDG PERIMETER WALKS SHALL BE CONCRETE OR PAVER BRICKS

 9. A CITY OF MARCO ISLAND PERMIT TO PERFORM WORK AND/ OR MAINTENANCE IN THE PUBLIC RIGHT

 OF WAY IS REQUIRED FOR WORK WITHIN S. BARFIELD DR. AND SILVER SPRAY WAY.

 10. THE FOLLOWING FOOT DETAIL INDEX SHEETS APPLY FOR ALL ROW ITEMS:
- INDEX NO. 425-052 DITCH BOTTOM INLETS, INDEX NO. 522-002 - DETECTABLE WARNINGS / SIDEWALK RAMPS
- INDEX NO. 522-002 DETECTABLE WARNINGS / SIDEWALK IN INDEX NO. 522-003 - TURNOUTS INDEX NO. 711-001 - SPECIAL MARKING AREAS.

UTILITY NOTES:

- 1. ALL WATER DISTRIBUTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS
 2. ALL SEWAGE COLLECTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN
- ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS.

 3. ALL ON SITE WATER AND SEWER IS TO BE OWNED AND MAINTAINED BY THE OWNER, UNLESS OTHERWISE NOTED.
- 4. THE LOCATION OF EXISTING UTILITIES HAS BEEN PREPARED FROM THE MOST RELIABLE INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT GUARANTEED, THEREFORE

 5. THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE
- FIELD PRIOR TO TO THE START OF ANY CONSTRUCTION ACTIVITIES.

 6. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL UTILITY LINES AND SERVICES DAMAGED DURING CONSTRUCTION, INCLUDING IRRIGATION LINES AND SERVICES. THE

 7. APPROPRIATE UTILITY SHALL BE NOTIFIED OF ALL DAMAGED LINES PRIOR TO REPAIR. ALL
- 8. ALL UTILITY CONNECTIONS SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE AGENCY HAVING JURISDICTION OF EACH INDIVIDUAL UTILITY LINE.
 9. THE UNDERGROUND CONTRACTOR SHALL MINIMIZE THE WORK AREA AND WIDTH OF TRENCHES TO AVOID DISTURBANCES OF NATURAL VEGETATION. SPOIL FROM TRENCHES

NECESSARY REPAIRS SHALL BE PERFORMED IMMEDIATELY UPON DAMAGED OF LINE.

- SHALL BE PLACED ONLY ON PREVIOUSLY CLEARED AREAS OR AS DIRECTED BY THE ENGINEER OR COUNTY.

 10. THE CONTRACTOR SHALL NOT REMOVE OR DISTURBED ANY TREES OR SHRUBS WITHOUT
- PRIOR APPROVAL FROM THE ENGINEER OR THE CITY.

 11. ALL CONDUITS REQUIRED FOR UTILITY INSTALLATIONS SHALL BE INSTALLED PRIOR TO THE
- STREET/PAVEMENT CONSTRUCTION. 12. SITE LIGHTING SHALL BE PROVIDED, AS REQUIRED, BY ARCHITECT.
- 13. ALL SEWER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF MARCO ISLAND PUBLIC UTILITIES DIVISION SPECIFICATIONS, LATEST EDITION.

 13. ALL WATER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH THE CITY OF MARCO
- ISLAND UTILITIES SPECIFICATIONS AND STANDARDS MANUAL, LATEST EDITION.

 14. CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE
- 14. CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE PROPERTY SIDE OF THE WATER SERVICE BACKFLOW PREVENTER.

 15. THE DEVELOPER'S CONTRACTOR TO PROVIDE BOLLARDS TO PROTECT FIRE HYDRANTS, AS NECESSARY.
- 16. THE DEVELOPER'S CONTRACTOR TO PROVIDE A MINIMUM OF 10 FEET SEPARATION
 DISTANCE BETWEEN WATER SERVICES AND ELECTRICAL BOXES AND STREET LIGHTS.
 17. ALL COSTS AND EXPENSES OF ANY AND ALL REPAIRS, REPLACEMENTS, MAINTENANCE
 AND RESTORATIONS OF ABOVEGROUND IMPROVEMENTS PERMITTED WITHIN A CUE SHALL
 BE THE SOLE FINANCIAL RESPONSIBILITY OF THE GRANTOR, ITS SUCCESSORS OR ASSIGNS

TRE:

- 1. A SEPARATE PERMIT IS REQUIRED PRIOR TO INSTALLATION OF ANY FIRE LINE.
 2. INSTALLATION OF ALL UNDERGROUND FIRE LINES SHALL COMPLY WITH THE 2022 EDITION
- OF NFPA 24. 3. UNDERGROUND FIRE LINES SHALL BE INSTALLED BY AN APPROPRIATELY CERTIFIED
- SPRINKLER CONTRACTOR OR A TYPE V UNDERGROUND CONTRACTOR AS DEFINED BY AND OUTLINED F.S. 633.102(24)

 4. PIPING USED FOR FIRE PROTECTION SERVICE THAT IS RUN UNDER DRIVEWAYS SHALL BE BURIED AT A MINIMUM DEPTH OF 36".

SITE SUMMARY TABLE					
TOTAL SITE			1.64	AC	
PROPOSED SITE SUMMARY			_		
BUILDING FOOTPRINT	27,900	SF	0.64	AC	39.1%
SIDEWALK/CONCRETE	1,780	SF	0.04	AC	2.5%
PAVEMENT (VUA)	18,900	SF	0.43	AC	26.5%
IMPERVIOUS SUBTOTAL	48,580	SF	1.12	AC	68.0%
OPEN SPACE:					
GREEN SPACE	16,493	SF	0.38	AC	23.1%
DRY DETENTION	6,365	SF	0.15	AC	8.9%
PERVIOUS SUBTOTAL	22,858	SF	0.52	AC	32.0%
TOTAL	71,438	SF	1.64	AC	100%

SETBACK TABLE (CITY OF MARCO ISLAND)									
PROPOSED BUILDING		PROVIDED	REQUIRED						
HEIGHT	(25' 5"	50'						
SETBACK FROM	I PROPERTY	LINE-BUILDING 1							
FRONT	E =	50.3'	50'						
REAR	W =	20.1'	15'						
SIDE	N =	N/A	15'						
SIDE	S =	15.1'	15'						
SETBACK FROM	PROPERTY	LINE-BUILDING 2							
FRONT	E =	50.3'	50'						
REAR	W =	20.4'	15'						
SIDE	N =	N/A	15'						
SIDE	S =	N/A	15'						
SETBACK FROM	PROPERTY	LINE-BUILDING 3							
FRONT	E =	50.4'	50'						
REAR	W =	20.0'	15'						
SIDE	N =	15.0'	15'						
SIDE	S =	N/A	15'						
BUILDING SEPA	RATION								
BUILDINGS 1&2		50'							
BUILDINGS 2&3		50'							

ZONING SUMMARY	LAND USE	
SITE	C-3	VACANT
NORTH	C-3	OCCUPIED
SOUTH	C-3	OCCUPIED
EAST	ROW S. BARFIELD DR.	PUBLIC
EAST OF ROW	C-3 <u></u>	OCCUPIED
WEST	R/W SILVER SPRAY WAY	PUBLIC
WEST OF ROW	RSF-4	OCCUPIED

PARKING TABLE

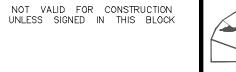
USE:STORAGE FACILITY: SELF-SERVICE

	RATE	SF	TOTAL
PARKING REQUIRED: PROPOSED BLDG	1 per 20,000 SF of storage buildings	27,809	1.4
	Plus 1 per 50 vehicle	20	0.4
	Minimum of 4.		
TOTAL REQUIRED			4
PARKING PROVIDED	1 garage space within each unit		20
HANDICAP REQUIRED			1
HANDICAP PROVIDED			2
LOADING REQUIRED	74' X 10' FOR SELF-STORAGE		2
LOADING PROVIDED			2

NOTE: DRY FLOOD PROOFING IS PROPOSED TO 11.17' NAVD

	"E 180.00' (P) "E 180.00' (C) SETBACK 15.0' SETBACK 15.0' SETBACK 50.0' (C) SETBACK 5	0 15 30 60 SCALE IN FEET
PROPOSED STOP SIGN R1-1 AND 24" STOP BAR (TYP) PROPOSED DO NOT ENTER" SIGN R5-1 PROPOSED 2 VALLEY GUTTER EACH UNIT PROVIDES A A (GARAGE PARKING SPACE) LANDSCAPE BUFFER 5 0 - 1 SETBACK 20.1 A (SO W) SETBACK 20.1 SETBACK 20.1 A (SO W) SETBACK 20.1 SETBACK 20.1 A (SO W) SETBACK 20.1 A (SO W) SETBACK 20.1 A (SO W) SETBACK 20.1 SETBACK 20	DETECTABLE WA 2 PER ACCESS - FDOT INDEX NO. 6 R12.0'	
C-3	330 b 50.00° (b)	PROPOSED ASPHALT DRY DETENTION PROPOSED 4" CONC. EXISTING GRADE XX PROPOSED GRADE MITERED END SECTION CATCH BASIN
LLC CLIENT: MARCO LUXE LLC MARCO LUXE	SITE PLAN	PROJ NO. PROJ NO. SCALE 1" = 30'

CHECKED		1	6/27/24	City of Marco Comment Letter dated 6/11/24	BAF	-
CHECKED_	BAF	2				
DRAWN	AOM	3				
DESIGN		4				
DESIGN	BAF	5				

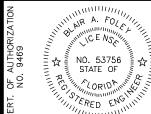


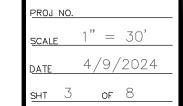
Blair A. Foley, P.E., LLC

Civil Engineer / Development Consultant
120 Edgemere Way South - Naples, Fl. 34105
Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols000@aol.com

MARCO LUXE Collier County, Florida

SITE PLAN





GENERAL NOTES

1. ELEVATIONS ARE REFERENCED TO NAVD OF 1988.

THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZED THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EXISTING EASEMENTS OF RECORD.

4. HANDICAP PARKING AND ACCESSIBILITY SHALL BE IN ACCORDANCE WITH THE LATEST STATE OF

FLORIDA REQUIREMENTS OR THE AMERICANS WITH DISABILITY ACT (ADA) WHICHEVER IS MORE STRINGENT. 5. ALL DRIVEWAYS, LANDSCAPING, SIGNS, GRASS, ETC. SHALL BE RESTORED TO A CONDITION EQUIVALENT TO PRE CONSTRUCTION CONDITION UNLESS OTHERWISE APPROVED BY THE ENGINEER AND

6. REQUESTS FOR INSPECTIONS SHALL BE PROVIDED TO CITY OF MARCO ISLAND STAFF AT LEAST FORTY EIGHT (48) HOURS PRIOR TO THE REQUESTED INSPECTION.

7. THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE COUNTY AND ALL UTILITY COMPANIES IN THE AREA 48 HOURS MINIMUM PRIOR TO START OF CONSTRUCTION, AND SHALL HAVE ALL EXISTING UTILITY LINES (SPRINT, FPL, WATER, CABLE, SANITARY, SEWER, IRRIGATION, FORCE MAIN & ANY OTHERS) LOCATED AND FLAGGED PRIOR TO ANY EXCAVATION.

8. CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ENGINEER PRIOR TO CONSTRUCTION.

9. THE CONTRACTOR IS REQUIRED TO OBTAIN WRITTEN APPROVAL FROM THE ENGINEER FOR ANY DEVIATIONS FROM THE APPROVED PLANS AND/OR SPECIFICATIONS.

10. CONTRACTOR SHALL PROMPTLY REPORT ALL FIELD CHANGES TO THE ENGINEER.

11. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS, ALL EXISTING CONFLICTS TO DESIGN PLANS (I.E. POLES, BOXES, VALVES ETC.), AND ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY REQUIRED PLAN DEVIATIONS. CONTRACTOR IS SOLELY RESPONSIBLE FOR ALL DAMAGES AS A RESULT OF ALL CONFLICTS NOT SATISFACTORLY RESOLVED. ENGINEER IS NOT LIABLE FOR ANY CONFLICT SHOWN OR OMMITED FROM THIS PLAN OR SURVEY.

12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC AND USAGE OF THE EXISTING STREETS ADJACENT TO THE PROJECT SITE. ALL TRAFFIC MAINTENANCE CONTROL SHALL BE IN ACCORDANCE WITH THE FLORIDA MANUAL OF TRAFFIC AND SAFE PRACTICES FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, AND UTILITY OPERATIONS.

13. ALL DISTURBED PERVIOUS AREAS NOT LANDSCAPED SHALL BE SODDED AND/OR SEEDED AND MULCHED AS DIRECTED BY THE OWNER OR ENGINEER.

14. DIMENSIONS WHICH REFERENCE CURBING ARE SHOWN TO FACE OF CURB UNLESS OTHERWISE NOTED. 15. ALL CONSTRUCTION METHODS AND MATERIALS TO CITY OF MARCO ISLAND LAND DEVELOPMENT CODE

AND F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION.

16. SEE CLEARING PLAN FOR LIMITS OF CLEARING.

17. THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE INFRASTRUCTURE, OPEN SPACES, & PAVED AREAS ON THEIR PROPERTY.

18. FINAL RESTORATION UPON REMOVAL OF TEMPORARY CONSTRUCTION ENTRANCE; UPON COMPLETION OF THE PROJECT, THE TEMPORARY CONSTRUCTION ENTRANCE AND ASSOCIATED IMPROVEMENTS SHALL BE REMOVED AND THE RIGHT-OF-WAY, INCLUDING GROUND COVER SHALL BE RESTORED

1. ALL EXOTIC VEGETATION AS DEFINED BY THE CITY OF MARCO ISLAND L.D.C. SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE RESPONSIBILITY OF THE OWNER.

2. THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR THE MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM. 3. CONTRACTOR SHALL REMOVE LIMEROCK WITHIN LANDSCAPE ISLANDS LOCATED WITHIN AND ADJACENT TO THE PARKING AREAS. THESE AREAS SHALL FILLED WITH CLEAN DIRT OR AS DIRECTED BY THE LANDSCAPE ARCHITECT.

PAVING, GRADING AND DRAINAGE NOTES:

THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE SURFACE WATER MANAGEMENT SYSTEM AS OUTLINED BY THE DESIGN ENGINEER ON THESE

2. ALL ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM (NAVD) 1988. 3. MIN. FINISHED FLOOR ELEVATIONS SHALL BE AS SHOWN ON THE SITE PLAN.

4. SITE IS LOCATED WITHIN THE FLOOD ZONE AE. . HAY BALES, SILT SCREENS AND OTHER EROSION CONTROL MEASURES SHALL BE UTILIZED TO CONTAIN ALL DISTURBED AREAS IN ACCORDANCE WITH THE EROSION CONTROL DETAILS. 6. SILT SCREENS TO BE INSTALLED AROUND PERIMETER OF PROPERTY AND/OR DISTURBED AREAS AND

MAINTAINED DURING CONSTRUCTION. 7. ALL PROPOSED ON-SITE PAVING SHALL BE ASPHALT, UNLESS OTHERWISE NOTED.

-8. NQTE-ALL BLOG. PERIMETER-WALKS-SHALL BE-CONGRETE OB-PAYER BRICKS. 9. A CITY OF MARCO ISLAND PERMIT TO PERFORM WORK AND/ OR MAINTENANCE IN THE PUBLIC RIGHT OF WAY IS REQUIRED FOR WORK WITHIN S. BARFIELD DR. AND SILVER SPRAY WAY. 10. THE FOLLOWING FOOT DETAIL INDEX SHEETS APPLY FOR ALL ROW HEMS:

INDEX NO. 425-052 - DITCH BOTTOM INLETS, INDEX NO. 522-002 - DETECTABLE WARNINGS / SIDEWALK RAMPS

INDEX NO. 522-003 - TURNOUTS INDEX NO. 711-001 - SPECIAL MARKING AREAS.

STREET/PAVEMENT CONSTRUCTION.

BURIED AT A MINIMUM DEPTH OF 36".

ALL WATER DISTRIBUTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH WATER AND SEWER DEPT., MANUAL OF STANDARDS AND SPECIFICATIONS, LATEST EDITION.

ALL SEWAGE COLLECTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN

ACCORDANCE WITH WATER AND SEWER DEPT., MANUAL OF STANDARDS AND SPECIFICATIONS, LATEST EDITION. ALL ON SITE WATER AND SEWER IS TO BE OWNED AND MAINTAINED BY THE OWNER, UNLESS OTHERWISE NOTED. THE LOCATION OF EXISTING UTILITIES HAS BEEN PREPARED FROM THE MOST RELIABLE

INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT GUARANTEED, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE FIELD PRIOR TO TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL UTILITY LINES AND SERVICES DAMAGED DURING CONSTRUCTION, INCLUDING IRRIGATION LINES AND SERVICES. THE APPROPRIATE UTILITY SHALL BE NOTIFIED OF ALL DAMAGED LINES PRIOR TO REPAIR. ALL NECESSARY REPAIRS SHALL BE PERFORMED IMMEDIATELY UPON DAMAGED OF LINE.

ALL UTILITY CONNECTIONS SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE AGENCY HAVING JURISDICTION OF EACH INDIVIDUAL UTILITY LINE. THE UNDERGROUND CONTRACTOR SHALL MINIMIZE THE WORK AREA AND WIDTH OF TRENCHES TO AVOID DISTURBANCES OF NATURAL VEGETATION. SPOIL FROM TRENCHES SHALL BE PLACED ONLY ON PREVIOUSLY CLEARED AREAS OR AS DIRECTED BY THE

ENGINEER OR COUNTY THE CONTRACTOR SHALL NOT REMOVE OR DISTURBED ANY TREES OR SHRUBS WITHOUT PRIOR APPROVAL FROM THE ENGINEER OR THE CITY. ALL CONDUITS REQUIRED FOR UTILITY INSTALLATIONS SHALL BE INSTALLED PRIOR TO THE

SITE LIGHTING SHALL BE PROVIDED, AS REQUIRED, BY ARCHITECT. 13. ALL SEWER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF MARCO ISLAND WATER AND SEWER DEPT., MANUAL OF STANDARDS AND SPECIFICATIONS, LATEST EDITION. 13. ALL WATER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH THE CITY OF MARCO

ISLAND UTILITIES SPECIFICATIONS AND STANDARDS MANUAL, LATEST EDITION. 14. CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE PROPERTY SIDE OF THE WATER SERVICE BACKFLOW PREVENTER.

15. THE DEVELOPER'S CONTRACTOR TO PROVIDE BOLLARDS TO PROTECT FIRE HYDRANTS, AS 16. THE DEVELOPER'S CONTRACTOR TO PROVIDE A MINIMUM OF 10 FEET SEPARATION

DISTANCE BETWEEN WATER SERVICES AND ELECTRICAL BOXES AND STREET LIGHTS. ALL COSTS AND EXPENSES OF ANY AND ALL REPAIRS, REPLACEMENTS, MAINTENANCE AND RESTORATIONS OF ABOVEGROUND IMPROVEMENTS PERMITTED WITHIN A CUE SHALL BE THE SOLE FINANCIAL RESPONSIBILITY OF THE GRANTOR, ITS SUCCESSORS OR ASSIGNS

A SEPARATE PERMIT IS REQUIRED PRIOR TO INSTALLATION OF ANY FIRE LINE. 2. INSTALLATION OF ALL UNDERGROUND FIRE LINES SHALL COMPLY WITH THE 2022 EDITION UNDERGROUND FIRE LINES SHALL BE INSTALLED BY AN APPROPRIATELY CERTIFIED SPRINKLER CONTRACTOR OR A TYPE V UNDERGROUND CONTRACTOR AS DEFINED BY AND OUTLINED F.S. 633.102(24)

PIPING USED FOR FIRE PROTECTION SERVICE THAT IS RUN UNDER DRIVEWAYS SHALL BE

2. CURRENT ZONING FOR THIS PARCEL IS C-3.

NOTE: FIRE HYDRANT MUST BE CLOW MEDALLION

FIRE METER: 6" NEPTUNE C&I MACH 10 R900, 2.0 TO 2,000 US GPM, FIRE FLOW 1,800 US GPM

FIRE FLOW REQUIREMENTS

FBC TYPE IIB, SPRINKLED

FLOW REQUIREMENT 75% RED. MIN. REQUIREMENT Building 1: 6,909 SF 1,750 GPM 2 HR 438 GPM 1,000 GPM 2,500 GPM 2 HR Building 2: 13.991 SF 625 GPM 1,000 GPM Building 3: 6,909 SF 1,750 GPM 2 HR 438 GPM 1,000 GPM

According to NFPA Table 18.4.5.2.1 Fire Flow Requirement,

75% Reduction Allowed for Sprinkled Buildings OR 1000 GPM Min = **1,000 GPM**

NOTE: SEE CITY OF MARCO ISLAND APPROVED PRODUCTS LIST, APPENDIX I, CITY OF MARCO ISLAND WATER & SEWER DEPARTMENT. MANUAL OF STANDARD AND SPECIFICATION

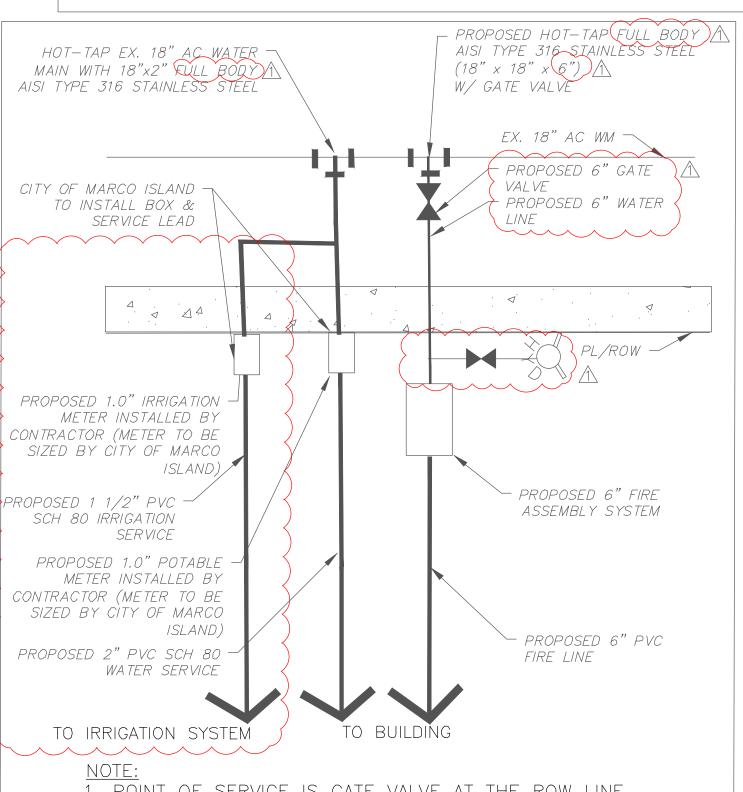
NOTE: POTABLE AND IRRIGATION FLOW METER MANUFACTURER MUST BE "NEPTUNE" R900VI RADIO READ AND RECORD IN GALLONS.

NOTE: BACK FLOW PREVENTION DEVICE MANUFACTURERS MAY BE: AMES/WATTS 400B/919, WATTS 909-009 SERIES, WILKENS 975,

FIRE BFP MAY BE AMES/WATTS C300/M300, WATTS 709, WILKENS 350 OR 450 N-SHAPES. (ALL MUST BE RPZ)

SEE CMI MANUAL OF STANDARDS APPENDIX I-6

NOTE: EXISTING AC WATER MAIN IS +/- 18" OUTSIDE DIAMETER. THE OD IS KNOWN TO VARY. EXCAVATE AND MEASURE THE OD AT THE LOCATION OF THE PROPOSED CONNECTION TO CONFIRM THE CORRECT SIZE FOR THE HOT TAP PARTS.

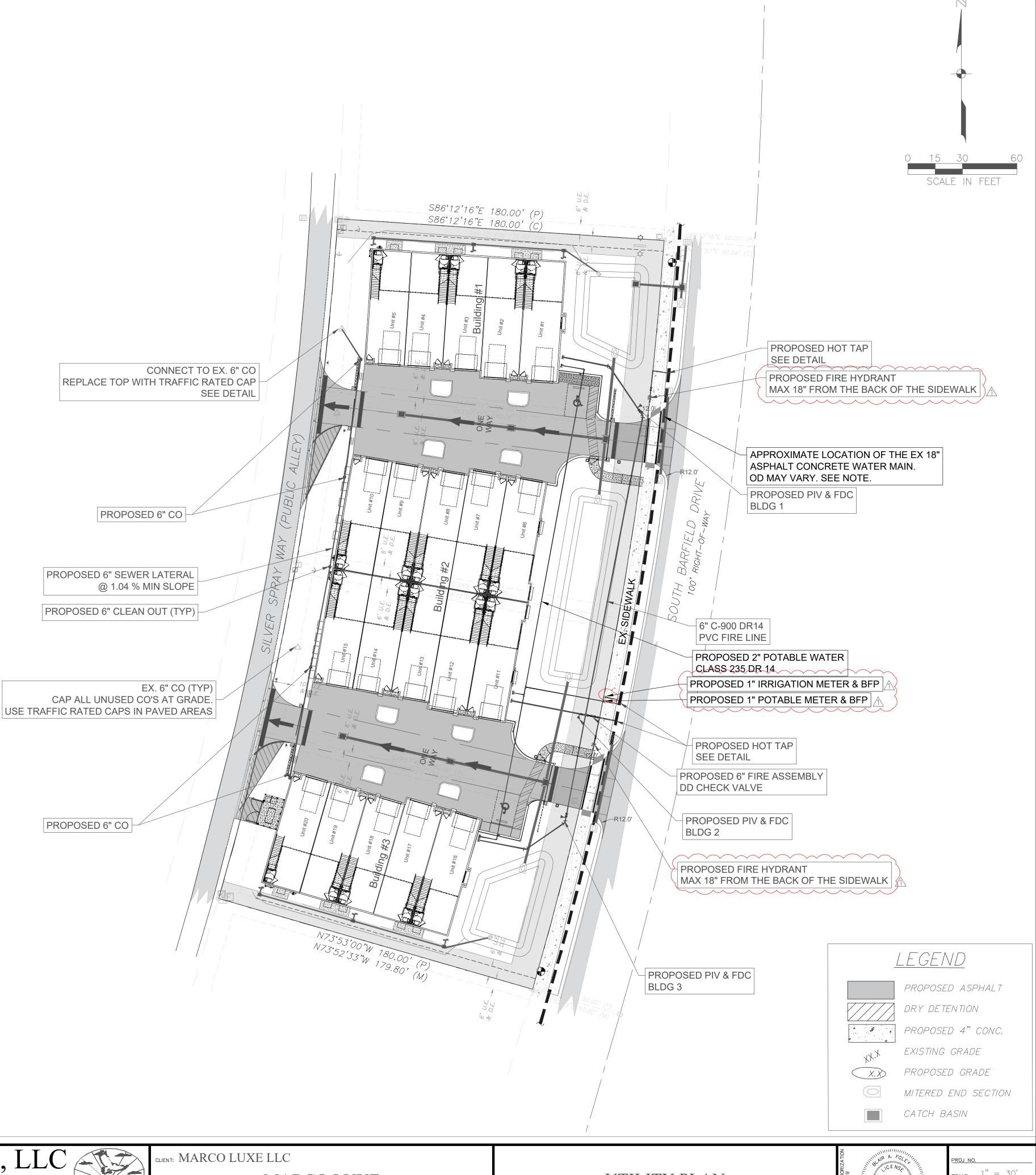


. POINT OF SERVICE IS GATE VALVE AT THE ROW LINE TO OBTAIN O.D. OF PIPE THEN PLACE ORDER THEN MAKE SURE THE SAME SPOT IS EXCAVATED OR OD MAY CHANGE DURING riangle

HOT-TAP WATER / METER CONNECTION DETAIL NTS

NOTE: LATERALS SHALL HAVE A MINIMUM DEPTH OF 30" AND A MAXIMUM DEPTH OF 48" BELOW FINISHED GRADE. IN LOCATIONS WHERE A MINIMUM DEPTH OF 30" CANNOT BE PROVIDED, LATERALS SHALL BE C900, DR 14 PVC PIPE. AT NO TIME SHALL THE DEPTH OF A LATERAL BE LESS THAN 24".

ADDITIONALLY, SEWER LATERALS SHOULD HAVE 30" (MIN.) TO 48" (MAX.) COVER AT THE CLEANOUT PER SECTION 3.1.4 OF OUR DESIGN CRITERIA AND AS SHOWN IN DETAIL WW-10.



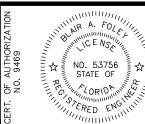
BAF AOM CHECKED___BAF 6/27/24 City of Marco Comment Letter dated 6/11/24 APPD BY DATE REVISION

NOT VALID FOR CONSTRUCTION UNLESS SIGNED IN THIS BLOCK

Blair A. Foley, P.E., LLC

Civil Engineer / Development Consultant 120 Edgemere Way South - Naples, Fl. 34105 Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols000@aol.com

MARCO LUXE Collier County, Florida UTILITY PLAN



1" = 30'4/9/2024

GENERAL NOTES

- 1. ELEVATIONS ARE REFERENCED TO NAVD OF 1988.
- 2. CURRENT ZONING FOR THIS PARCEL IS C-3.

3. THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZED THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EXISTING EASEMENTS OF RECORD.

4. HANDICAP PARKING AND ACCESSIBILITY SHALL BE IN ACCORDANCE WITH THE LATEST STATE OF FLORIDA REQUIREMENTS OR THE AMERICANS WITH DISABILITY ACT (ADA) WHICHEVER IS MORE STRINGENT.

5. ALL DRIVEWAYS, LANDSCAPING, SIGNS, GRASS, ETC. SHALL BE RESTORED TO A CONDITION EQUIVALENT TO PRE CONSTRUCTION CONDITION UNLESS OTHERWISE APPROVED BY THE ENGINEER AND

6. REQUESTS FOR INSPECTIONS SHALL BE PROVIDED TO CITY OF MARCO ISLAND STAFF AT LEAST FORTY EIGHT (48) HOURS PRIOR TO THE REQUESTED INSPECTION.

7. THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE COUNTY AND ALL UTILITY COMPANIES IN THE AREA 48 HOURS MINIMUM PRIOR TO START OF CONSTRUCTION, AND SHALL HAVE ALL EXISTING UTILITY LINES (SPRINT, FPL, WATER, CABLE, SANITARY, SEWER, IRRIGATION, FORCE MAIN & ANY OTHERS) LOCATED AND FLAGGED PRIOR TO ANY EXCAVATION.

8. CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ENGINEER PRIOR TO CONSTRUCTION.

9. THE CONTRACTOR IS REQUIRED TO OBTAIN WRITTEN APPROVAL FROM THE ENGINEER FOR ANY DEVIATIONS FROM THE APPROVED PLANS AND/OR SPECIFICATIONS.

10. CONTRACTOR SHALL PROMPTLY REPORT ALL FIELD CHANGES TO THE ENGINEER.

11. THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS, ALL EXISTING CONFLICTS TO DESIGN PLANS (I.E. POLES, BOXES, VALVES ETC.), AND ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY REQUIRED PLAN DEVIATIONS. CONTRACTOR IS SOLELY RESPONSIBLE FOR ALL DAMAGES AS A RESULT OF ALL CONFLICTS NOT SATISFACTORLY RESOLVED. ENGINEER IS NOT LIABLE FOR ANY CONFLICT SHOWN OR OMMITED FROM THIS PLAN OR SURVEY.

12. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC AND USAGE OF THE EXISTING STREETS ADJACENT TO THE PROJECT SITE. ALL TRAFFIC MAINTENANCE CONTROL SHALL BE IN ACCORDANCE WITH THE FLORIDA MANUAL OF TRAFFIC AND SAFE PRACTICES FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, AND UTILITY OPERATIONS.

13. ALL DISTURBED PERVIOUS AREAS NOT LANDSCAPED SHALL BE SODDED AND/OR SEEDED AND MULCHED AS DIRECTED BY THE OWNER OR ENGINEER.

14. DIMENSIONS WHICH REFERENCE CURBING ARE SHOWN TO FACE OF CURB UNLESS OTHERWISE NOTED.

15. ALL CONSTRUCTION METHODS AND MATERIALS TO CITY OF MARCO ISLAND LAND DEVELOPMENT CODE AND F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION.

16. SEE CLEARING PLAN FOR LIMITS OF CLEARING.

17. THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE INFRASTRUCTURE, OPEN SPACES, & PAVED AREAS ON THEIR PROPERTY.

18. FINAL RESTORATION UPON REMOVAL OF TEMPORARY CONSTRUCTION ENTRANCE: UPON COMPLETION OF THE PROJECT, THE TEMPORARY CONSTRUCTION ENTRANCE AND ASSOCIATED IMPROVEMENTS SHALL BE REMOVED AND THE RIGHT-OF-WAY, INCLUDING GROUND COVER SHALL *BE RESTORED*

1. ALL EXOTIC VEGETATION AS DEFINED BY THE CITY OF MARCO ISLAND L.D.C. SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE

RESPONSIBILITY OF THE OWNER. 2. THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR THE MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM.

3. CONTRACTOR SHALL REMOVE LIMEROCK WITHIN LANDSCAPE ISLANDS LOCATED WITHIN AND ADJACENT TO THE PARKING AREAS. THESE AREAS SHALL FILLED WITH CLEAN DIRT OR AS DIRECTED BY THE LANDSCAPE ARCHITECT.

PAVING, GRADING AND DRAINAGE NOTES:

. THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE SURFACE WATER MANAGEMENT SYSTEM AS OUTLINED BY THE DESIGN ENGINEER ON THESE

- 2. ALL ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM (NAVD) 1988. 3. MIN. FINISHED FLOOR ELEVATIONS SHALL BE AS SHOWN ON THE SITE PLAN.
- 4. SITE IS LOCATED WITHIN THE FLOOD ZONE AE. 5. HAY BALES, SILT SCREENS AND OTHER EROSION CONTROL MEASURES SHALL BE UTILIZED TO CONTAIN ALL DISTURBED AREAS IN ACCORDANCE WITH THE EROSION CONTROL DETAILS.
- . SILT SCREENS TO BE INSTALLED AROUND PERIMETER OF PROPERTY AND/OR DISTURBED AREAS ANL
- MAINTAINED DURING CONSTRUCTION. 7. ALL PROPOSED ON-SITE PAVING SHALL BE ASPHALT, UNLESS OTHERWISE NOTED.

8. NOTE ALL BLDG. PERIMETER WALKS SHALL BE CONCRETE OF PAVER BRICKS. (9. A CITY OF MÁRCO ÍSLAŇD PERMÍT TO PĚRFÖRM WORK AND/ OR MAINTENÁNCE IN ŢHE PUBLIC RIGHT `

OF WAY IS REQUIRED FOR WORK WITHIN S. BARFIELD DR. AND SILVER SPRAY WAY. 10. THE FOLLOWING FOOT DETAIL INDEX SHEETS APPLY FOR ALL ROW ITEMS:

INDEX NO. 425-052 - DITCH BOTTOM INLETS, INDEX NO. 522-002 - DETECTABLE WARNINGS / SIDEWALK RAMPS

INDEX NO. 522-003 - TURNOUTS INDEX NO. 711-001 - SPECIAL MARKING AREAS.

UTILITY NOTES:

ALL WATER DISTRIBUTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS ALL SEWAGE COLLECTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS. ALL ON SITE WATER AND SEWER IS TO BE OWNED AND MAINTAINED BY THE OWNER,

UNLESS OTHERWISE NOTED. THE LOCATION OF EXISTING UTILITIES HAS BEEN PREPARED FROM THE MOST RELIABLE

INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT GUARANTEED, THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE

FIELD PRIOR TO TO THE START OF ANY CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL UTILITY LINES AND SERVICES

DAMAGED DURING CONSTRUCTION, INCLUDING IRRIGATION LINES AND SERVICES. THE APPROPRIATE UTILITY SHALL BE NOTIFIED OF ALL DAMAGED LINES PRIOR TO REPAIR. ALL NECESSARY REPAIRS SHALL BE PERFORMED IMMEDIATELY UPON DAMAGED OF LINE.

ALL UTILITY CONNECTIONS SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE AGENCY HAVING JURISDICTION OF EACH INDIVIDUAL UTILITY LINE.

THE UNDERGROUND CONTRACTOR SHALL MINIMIZE THE WORK AREA AND WIDTH OF TRENCHES TO AVOID DISTURBANCES OF NATURAL VEGETATION. SPOIL FROM TRENCHES SHALL BE PLACED ONLY ON PREVIOUSLY CLEARED AREAS OR AS DIRECTED BY THE

D. THE CONTRACTOR SHALL NOT REMOVE OR DISTURBED ANY TREES OR SHRUBS WITHOUT PRIOR APPROVAL FROM THE ENGINEER OR THE CITY.

1. ALL CONDUITS REQUIRED FOR UTILITY INSTALLATIONS SHALL BE INSTALLED PRIOR TO THE STREET/PAVEMENT CONSTRUCTION.

P. SITE LÍGHTING SHALL BE PROVIDED, AS REQUIRED, BY ARCHITECT. 13. ALL SEWER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF MARCO ISLAND

PUBLIC UTILITIES DIVISION SPECIFICATIONS, LATEST EDITION. 13. ALL WATER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH THE CITY OF MARCO

ISLAND UTILITIES SPECIFICATIONS AND STANDARDS MANUAL, LATEST EDITION. 14. CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE

PROPERTY SIDE OF THE WATER SERVICE BACKFLOW PREVENTER. 15. THE DEVELOPER'S CONTRACTOR TO PROVIDE BOLLARDS TO PROTECT FIRE HYDRANTS, AS

16. THE DEVELOPER'S CONTRACTOR TO PROVIDE A MINIMUM OF 10 FEET SEPARATION DISTANCE BETWEEN WATER SERVICES AND ELECTRICAL BOXES AND STREET LIGHTS. 17. ALL COSTS AND EXPENSES OF ANY AND ALL REPAIRS, REPLACEMENTS, MAINTENANCE

AND RESTORATIONS OF ABOVEGROUND IMPROVEMENTS PERMITTED WITHIN A CUE SHALL BE THE SOLE FINANCIAL RESPONSIBILITY OF THE GRANTOR, ITS SUCCESSORS OR ASSIGNS

A SEPARATE PERMIT IS REQUIRED PRIOR TO INSTALLATION OF ANY FIRE LINE. INSTALLATION OF ALL UNDERGROUND FIRE LINES SHALL COMPLY WITH THE 2022 EDITION UNDERGROUND FIRE LINES SHALL BE INSTALLED BY AN APPROPRIATELY CERTIFIED

SPRINKLER CONTRACTOR OR A TYPE V UNDERGROUND CONTRACTOR AS DEFINED BY AND PIPING USED FOR FIRE PROTECTION SERVICE THAT IS RUN UNDER DRIVEWAYS SHALL BE BURIED AT A MINIMUM DEPTH OF 36".

SITE SUMMARY TABLE										
TOTAL SITE	1.64	AC								
PROPOSED SITE SUMMARY			•							
BUILDING FOOTPRINT	27,900	SF	0.64	AC	39.1%					
SIDEWALK/CONCRETE	1,780	SF	0.04	AC	2.5%					
PAVEMENT (VUA)	18,900	SF	0.43	AC	26.5%					
IMPERVIOUS SUBTOTAL	48,580	SF	1.12	AC	68.0%					
OPEN SPACE:										
GREEN SPACE	16,493	SF	0.38	AC	23.1%					
DRY DETENTION	6,365	SF	0.15	AC	8.9%					
PERVIOUS SUBTOTAL	22,858	SF	0.52	AC	32.0%					
TOTAL	71,438	SF	1.64	AC	100%					

WATER MANAGEMENT SUMMARY	
SFWMD Permit No. (N/A, no existing ERP)	
NGVD - 1.31' = NAVD	
10-YEAR, 1-DAY RAINFALL (in)	7.2
25-YEAR, 3-DAY RAINFALL (in)	12.50
100-YEAR, 3-DAY RAINFALL (in)	16.00
PEAK 10-YEAR, 1-DAY STORM STAGE (FT-NAVD)	4.15
PEAK 25-YEAR, 3-DAY STORM STAGE (FT-NAVD)	4.61
PEAK 100-YEAR, 3-DAY ZERO DISCHARGE STORM STAGE (FT-NAVD)	7.12
FEMA BFE (FT-NAVD) ZONE AE	10.00
PROPOSED DESIGN CONDITIONS:	
FFE (FT NAVD)	8.75
FLOOD PROOFING (FT NAVD)	11.17
MIN PARKING LOT EL. (FT-NAVD)	4.15
MIN BERM (FT-NAVD)	4.70
MIN ROAD (FT-NAVD)	5.70
ON-SITE CONTROL ELEVATION (FT-NAVD)	1.20

WATER QUALITY DE	TENTION VC	LUME REQU	UIRED				
Basin Area =		1.64	ac				
First one (1) inch of rur	noff over basii	<u>n:</u>					
				1" *1.64 ac x 1'/12	2" =	0.14	ac ft
					•		
2.5 Inches Times the F	ercentage of	Imperviousne	ess:				
Site Area for Water Quality Treatment = Basin Area - Roof =				of =	1.00	ac	
Impervious Area for W	.Q. Treatmen	t = Site Area	for W.Q.	-Total Pervious Are	a =	0.47	ac
	Perce	ent Impervious	ıs = Imper	vious Area/Site Are	a =	47.50	%
Volume	to be Treate	d = 2.5" x Per	rcent Imp	ervious x Basin Are	a =	0.16	ac ft
					•		
2.5 Inch	nes Times the	e Percentage	e of Impe	rviousness Gover	ns:	0.162	ac ft
		25% red	duction for	using Dry Detentio	n =	0.12	ac ft
	150%	6 increase for	r discharg	e to Impaired Water	s =	0.18	ac ft

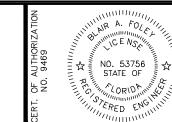
						0.30	AC FT		
			٦	Γotal	Volume =	13,212	CF		
		Swale Vo	lume fro	om 2	2.2' to 4.7' =	12,630	CF		
			Sı	vale	3 Volume =	3450	CF		
			Sı	vale	2 Volume =	5820	CF		
	1 Volume =	3360	CF						
Swale Storage									
			Total	Pipe	e Volume =	582	CF		
	403	LF	15	in	Volume =	495	CF		
	251	LF	8	in	Volume =	88	CF		
Pipe Storage									

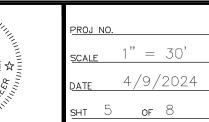
DRAINAGE TA	ABLE										
From	Structure Description	Invert El. (NAVD)	Grate El. (NAVD)	То	Structure Description	Invert El. (NAVD)	Grate El. (NAVD)	Length (FT)	Size (IN)	Pipe Material	Slope
CB 1	TYPE "C" BOX	5.0	8.0	CB 2	TYPE "C" BOX	4.5	8.0	57	15	RCP	0.88%
CB 2	TYPE "C" BOX	4.0	8.0	CB 3	TYPE "C" BOX	3.0	5.7	50	15	RCP	2.00%
CB 3	TYPE "C" BOX	3.0	5.7	MES 1	MITERED END	2.2		49	15	RCP	1.63%
CB 3	TYPE "C" BOX	3.0	5.7	MES 2	MITERED END	2.2		60	15	RCP	1.33%
CB 4	TYPE "C" BOX	5.0	8.0	CB 5	TYPE "C" BOX	4.5	8.0	57	15	RCP	0.88%
CB 5	TYPE "C" BOX	4.0	8.0	CB 6	TYPE "C" BOX	3.0	5.7	50	15	RCP	2.00%
CB 6	TYPE "C" BOX	3.0	5.7	MES 3	MITERED END	2.2		33	15	RCP	2.42%
CB 6	TYPE "C" BOX	3.0	5.7	MES 4	MITERED END	2.2		47	15	RCP	1.70%
CONTROL STRUCTURE	TYPE "C" BOX- CONT. STRUCTURE	1.6	4.0	CB 7	TYPE "C" BOX	1.6		22	15	HDPE	0.00%
CB 7	TYPE "C" BOX	1.6	3.3	EX. OUTFALL	TYPE "C" BOX	1.6	3.27	22	15	HDPE	0.05%
CB 7	TYPE "C" BOX	1.6	3.3	MES	MITERED END	1.6		9	15	HDPE	0.00%

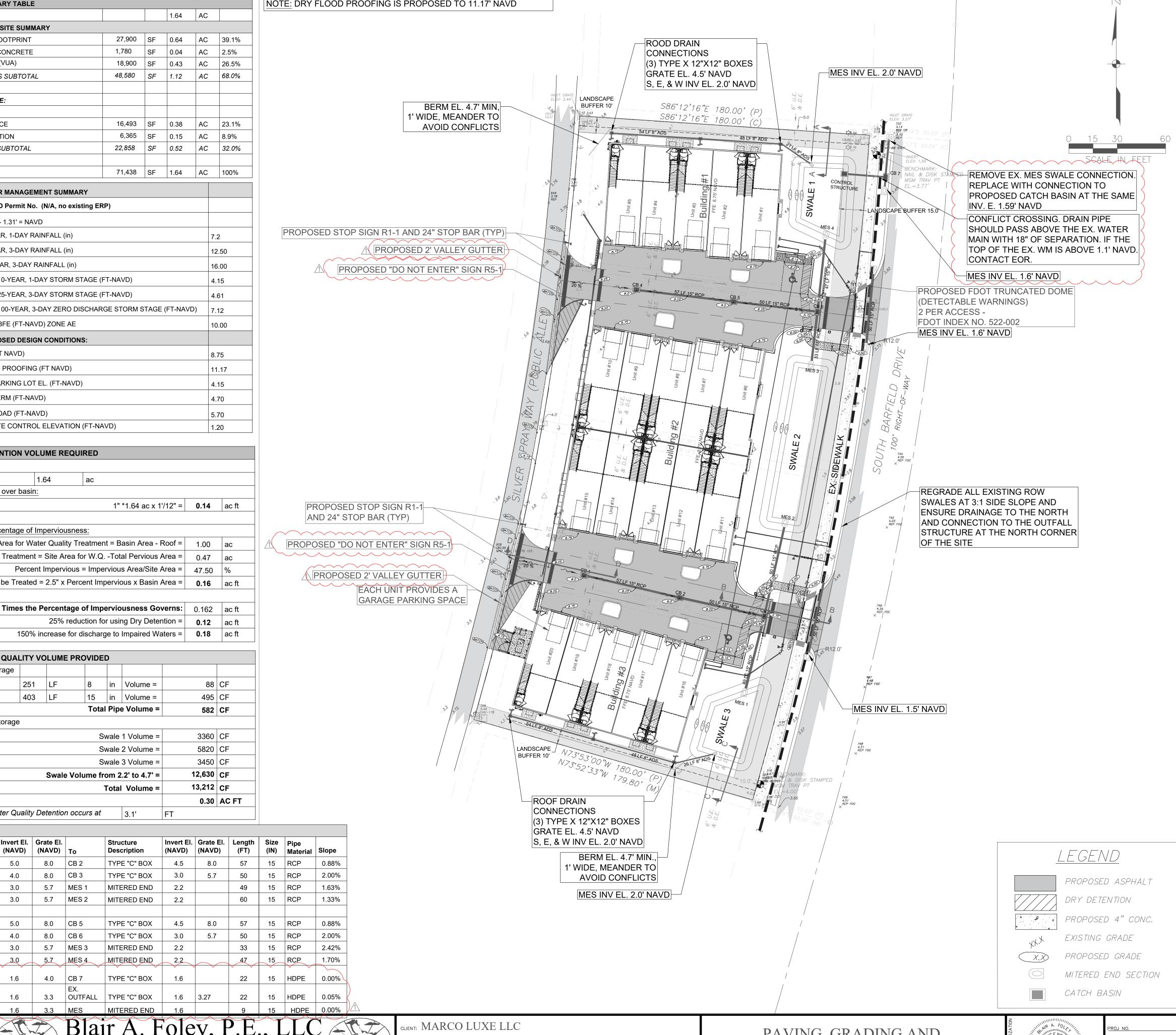
						001	
DESIGN	BAF	5			I		FOR CONSTRUCTION IED IN THIS BLOCK
<i>D</i> _0/0/\		4					
DRAWN	AOM	3					
CHECKED	BAF	2					
CHECKED_		1	6/27/24	City of Marco Comment Letter dated 6/11/24	BAF		
Q.C		NO.	DATE	REVISION	APPD BY		

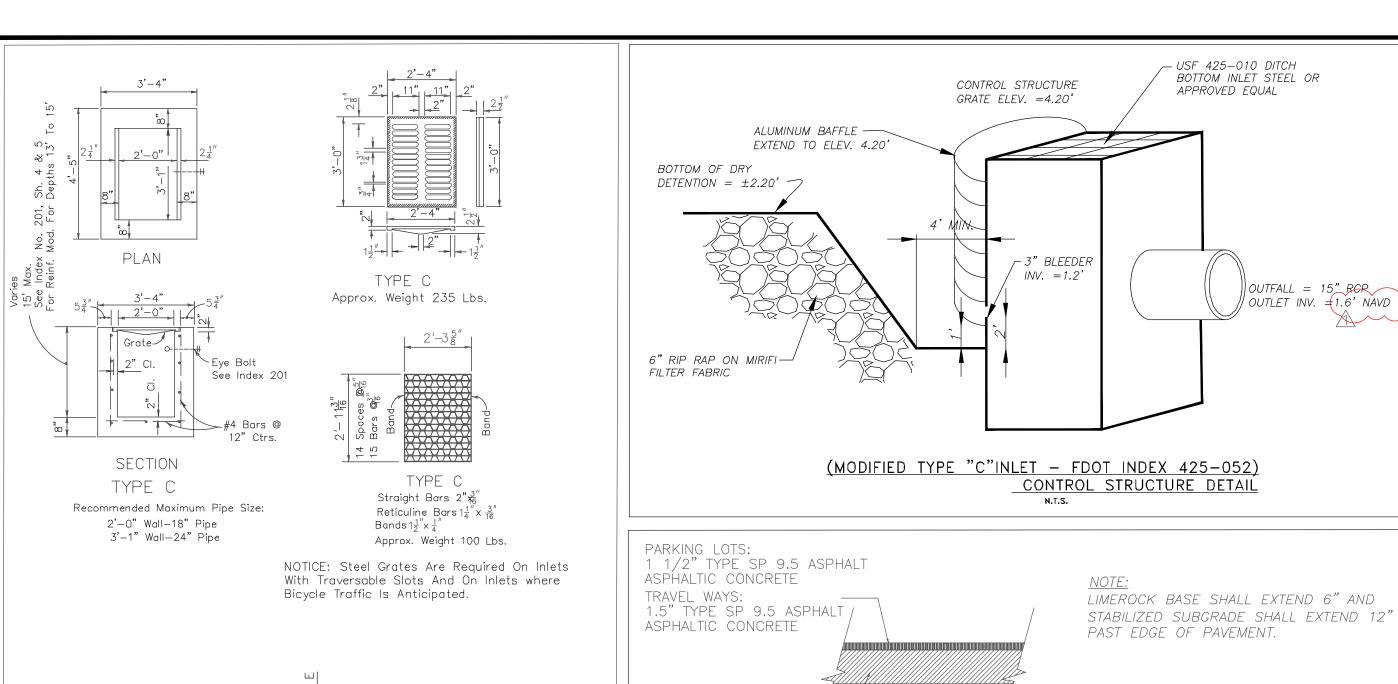
	Blair A. Foley, P.E., LLC	4
	Civil Engineer / Development Consultant	+
	120 Edgemere Way South - Naples, Fl. 34105	
Phone (23	39) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols000@	vaol.com

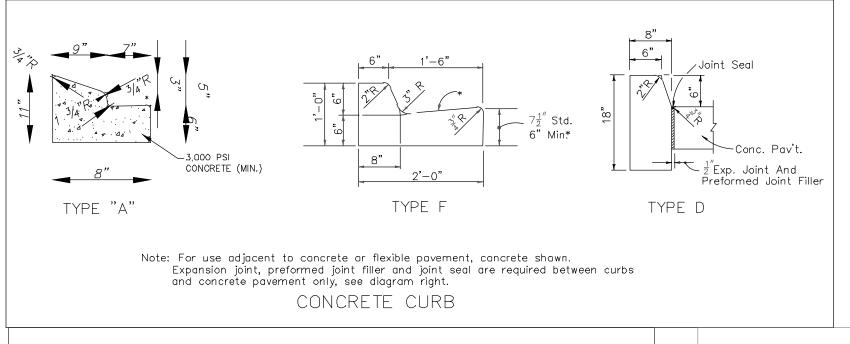
MARCO LUXE Collier County, Florida PAVING, GRADING AND DRAINAGE PLAN

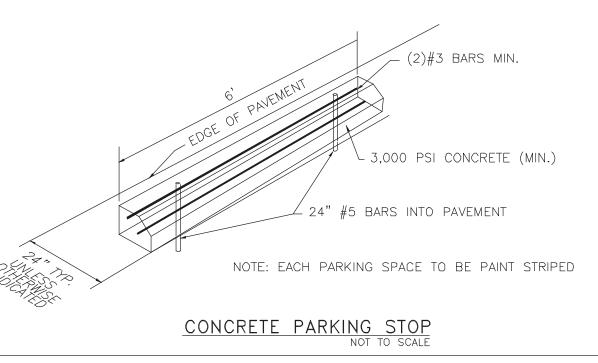


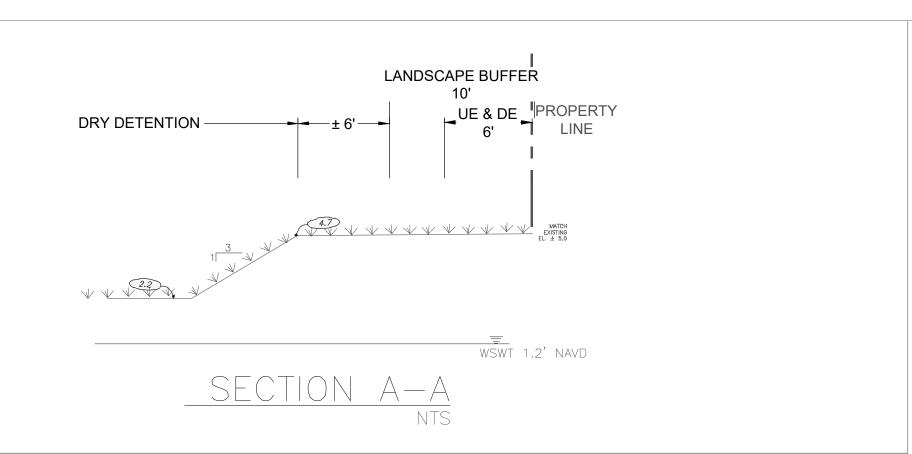


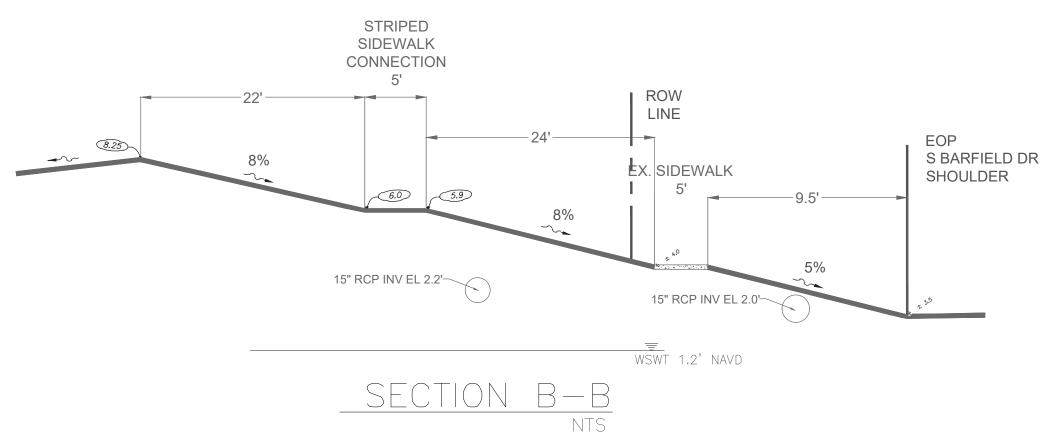


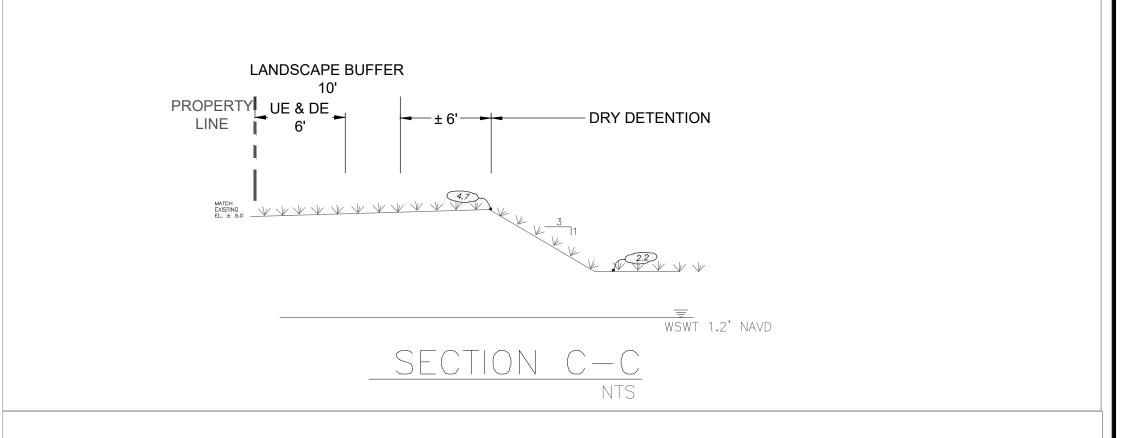


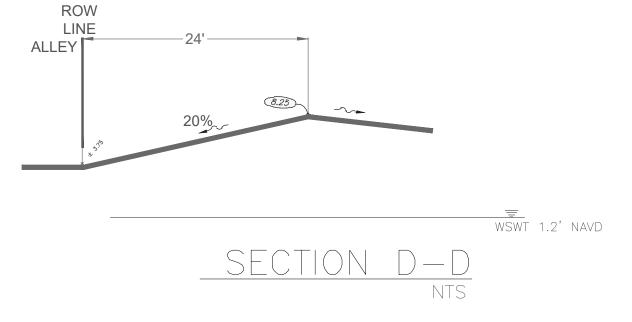


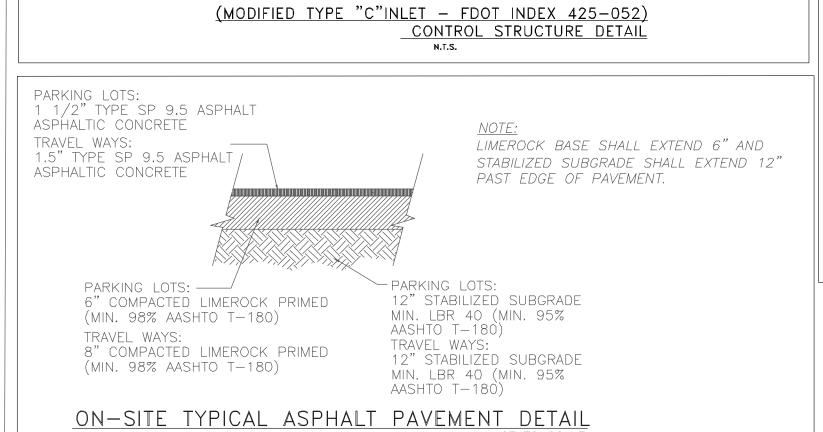


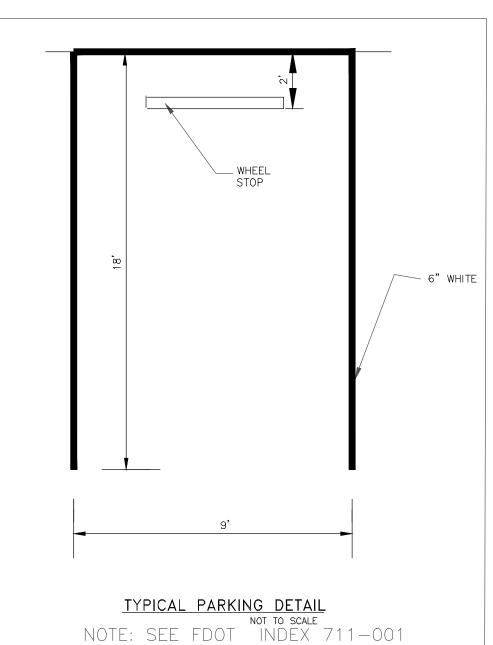




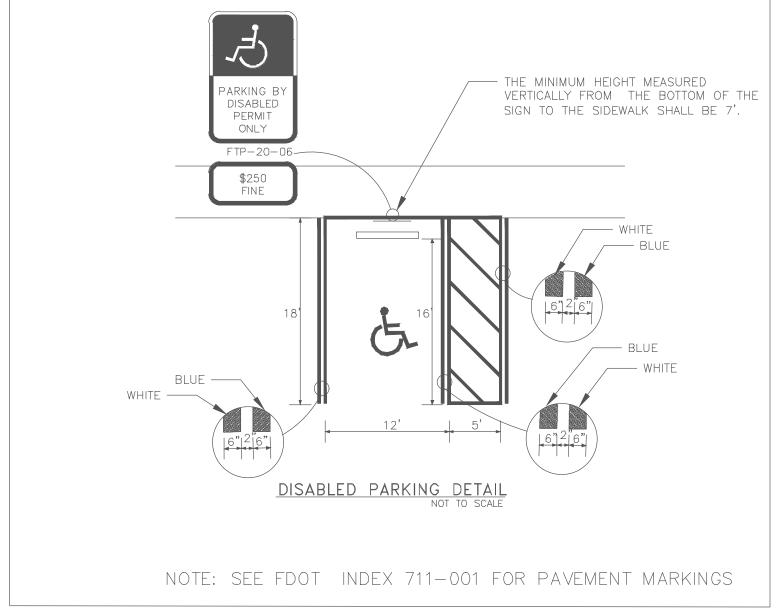


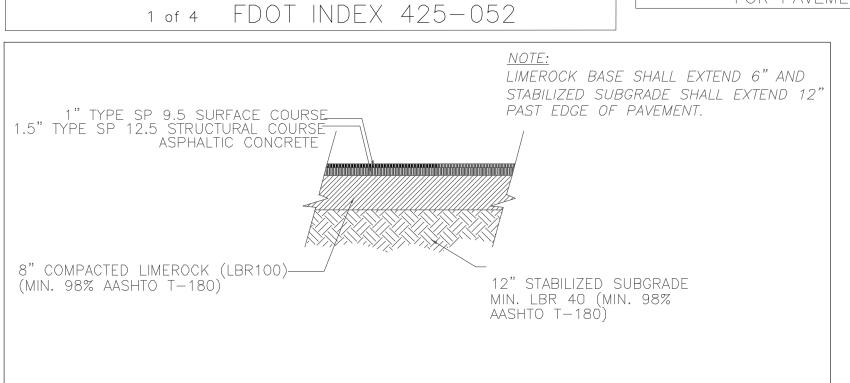






FOR PAVEMENT MARKINGS





Type D 3'-1"

Type E 3'-0"

3'-6"

Unless Otherwise Shown On Plans

NOTE: Alt. B Structure Bottom Only. See Index No. 200

STRUCTURE BOTTOM FOR INLETS TYPE C, D & E

GENERAL NOTES

1. These inlets are suitable for bicycle and pedestrian areas and are to be used in ditches,

2. Inlets subject to minimal debris should be constructed without slots. Where debris is a problem inlets should be constructed with slots. Slotted inlets located within roadway clear zones and in areas accessible to pedestrians shall have traversable slots. The traversable slot modification is not adaptable to inlet Type H. Slots may be constructed

3. Steel grates are to be used on all inlets where bicycle traffic is anticipated. Steel grates are to be used on all inlets with traversable slots. Either cast iron or steel grates may be used on inlets without slots where bicycle traffic is not anticipated. Either cast iron or steel grates may be used on all inlets with non-traversable slots. Subject to the selection described above, when Alternate G grate is specified in the plans, either the steel grate, hot dipped galvanized after fabrication, or the cast iron grate may be used, unless the

4. Recommended maximum pipe sizes shown are for concrete pipe. Pipe sizes larger than

6. Pavement to be used on inlets without slots and inlets with non-traversable slots only when called for in the plans; but required on all traversable slot inlets. Cost to be included in contract unit price for inlets. Quantities shown are for information only.

Traversable slots constructed in existing inlets shall be paid for as inlets partial, and shall include the cost for slot openings, paving and any required replacement grates.

8. Sodding to be used on all inlets not located in paved areas and paid for under contract

5. All exposed corners and edges of concrete are to be chamfered $\frac{3}{4}$.

medians and other areas subject to infrequent traffic loadings but are not to be placed

Type C 3'-1"

Type E 4'-6"

Unless Otherwise

Shown On Plans

at either or both ends as shown on plans.

those recommended must be checked for fit.

9. For supplementary details see Index No. 425—001.

DATE

plans stipulate the particular type.

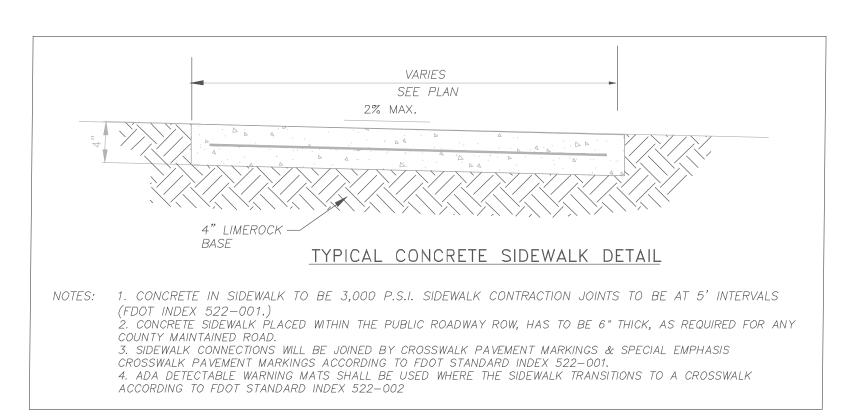
unit price for Sodding SY.

BAF

AOM

HECKED BAF

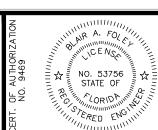






JENT: MARCO LUXE LLC MARCO LUXE Collier County, Florida

DETAILS & SECTIONS



CALE N.T.S. 4/9/2024



