

SITE DEVELOPMENT PLAN

Reviewed for PW
Z. Luff
07/08/2024
3:35:14 PM

**MARCO ISLAND
FIRE RESCUE**



Reviewed and approved for Fire Code Compliance pursuant to FFPC 1: 1.14

Plans must be on the job site at the time of inspection. Any errors or omissions in these plans shall be made to conform with applicable code requirements.

Daniel Zunzunegui, Fire Marshal
07/09/2024

Reviewed
MIU BB
7/3/24

A separate R/W permit is required for any work within the right-of-way.
ZLuff 07/08/24

Reviewed for code compliance
Joshua Ooyman
07/11/2024 9:13:14 AM

INDEX OF PLANS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	EROSION CONTROL / AERIAL
3	SITE DEVELOPMENT PLAN
4	WATER AND SEWER PLAN
5	PAVING GRADING AND DRAINAGE
6	DETAIL & SECTION SHEET
7	DETAIL SHEET
8	FIRE PLAN

Marco Luxe
138-168 S. BARFIELD DR.
MARCO ISLAND, FL 34145
S9/ T52S/ R26E

Prepared for Owner:
Marco Luxe LLC
1083 N Collier Blvd., #340
Marco Island, FL 34145

PROJECT DATA

PROJECT NAME: Marco Luxe
PROJECT NO.:
ZONING: C-3
FOLIO: 57190640006, 57190680008,
57190720007, 57190760009
FEMA FL. ZONE: AE 10.0' (2024 MAP)

BUILDING DATA

BUILDING CODE REFERENCES:
FLORIDA FIRE PREVENTION CODE 8TH ED.
FLORIDA BUILDING CODE 2023, 8TH ED.

BUILDING CODE DATA:

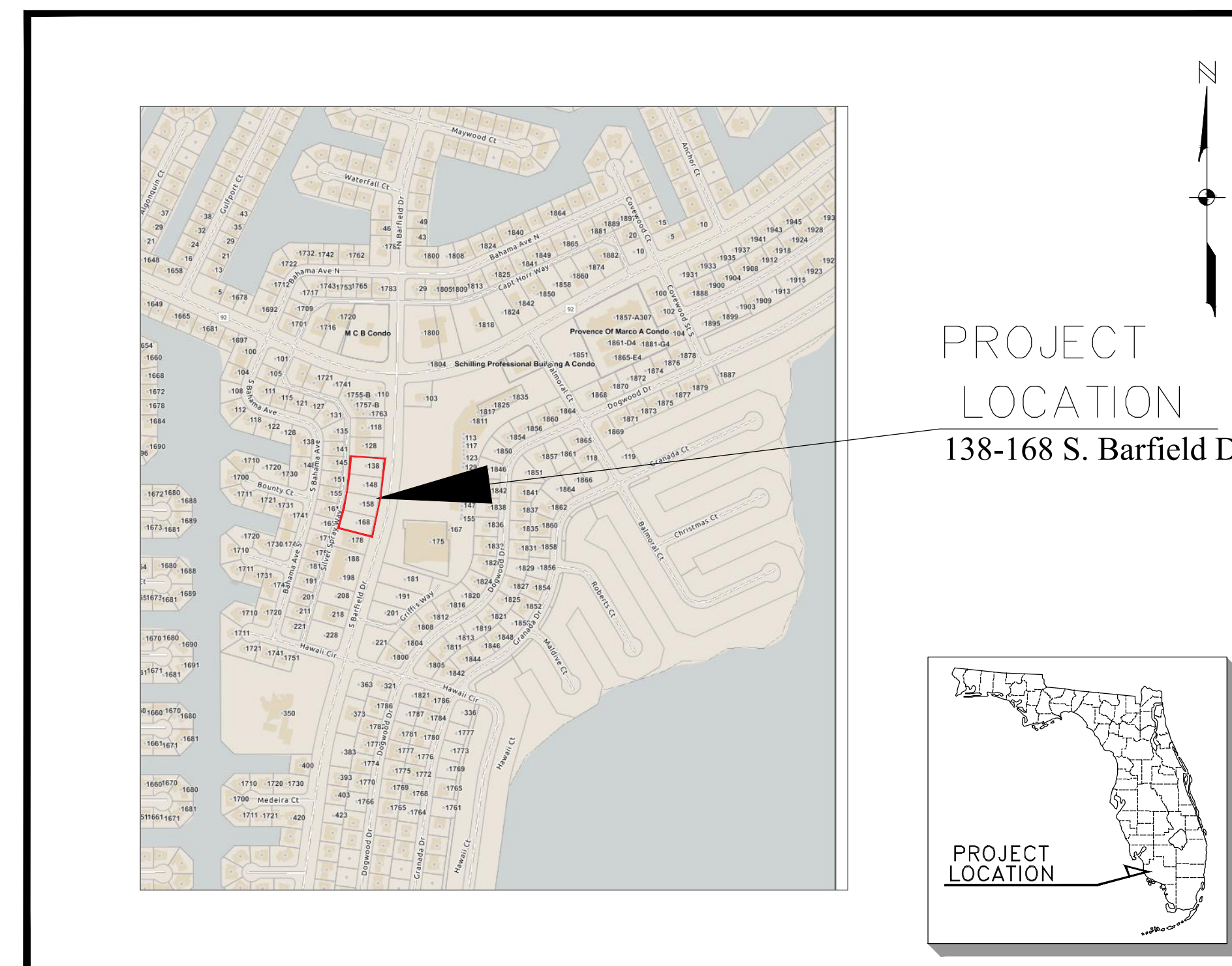
OCCUPANCY: STORAGE (S-1)
CONSTRUCTION TYPE: FBC TYPE IIB, SPRINKLED

BUILDING HEIGHT:
MAX HEIGHT: 50' HEIGHT (C-3)
PROPOSED HEIGHT: 2 STORIES, 23'-6"

BUILDING FLOOR AREA SUMMARY:

	PROPOSED
BUILDING 1 (AREA UNDER A/C)	6,909 SF
BUILDING 2 (AREA UNDER A/C)	13,991 SF
BUILDING 3 (AREA UNDER A/C)	6,909 SF
TOTAL (AREA UNDER A/C)	27,809 SF

BUILDING 1 COVERED ENTRY	55 SF
BUILDING 2 COVERED ENTRY	110 SF
BUILDING 3 COVERED ENTRY	55 SF



LEGAL DESCRIPTION

MARCO BCH UNIT 5 BLK 83 LOT S 7-10

DRAWING COORDINATION

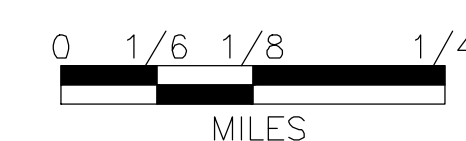
ARCHITECT: SheelyArchitects Inc
8200 College Parkway
Ft. Myers, FL 33919
(239) 482-2121



Blair A. Foley, P.E., LLC

Civil Engineer / Development Consultant
120 Edgemere Way South - Naples, FL 34105
Phone (239) 263-1222 - Cell (239) 289-4900 - Fax (239) 263-0472 - E-mail fols000@aol.com

LOCATION MAP

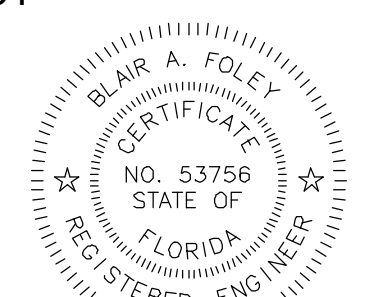


April 2024
Rev. July 2024

Blair A Foley Digitally signed by Blair A Foley
Date: 2024.07.02 14:39:51
-04'00'

Certificate Of Authorization No. 9469
Florida Registered Professional Engineer No. 53756

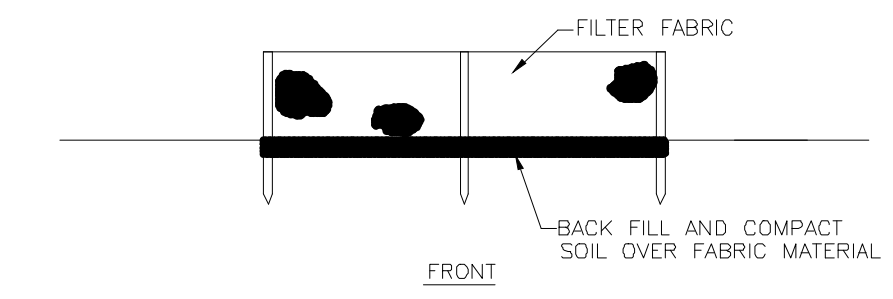
Name _____ Date _____



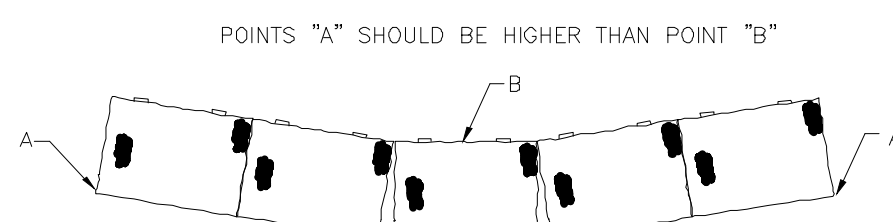
MARCO LUXE
-SDP

SHEET
1 of 8

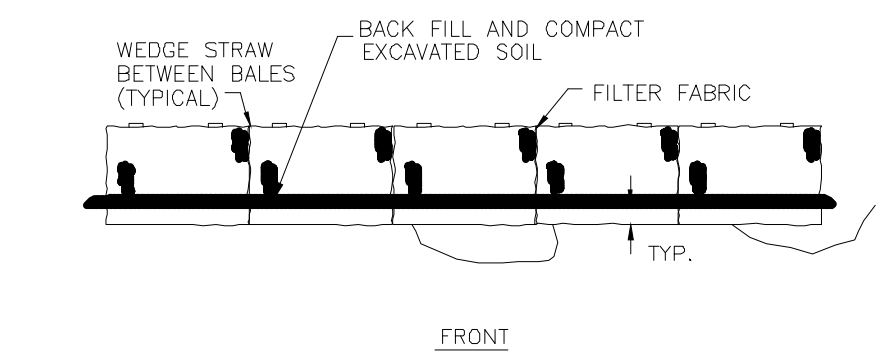
FLUCCS CODES:
190-DISTURBED/MOWED GRASS



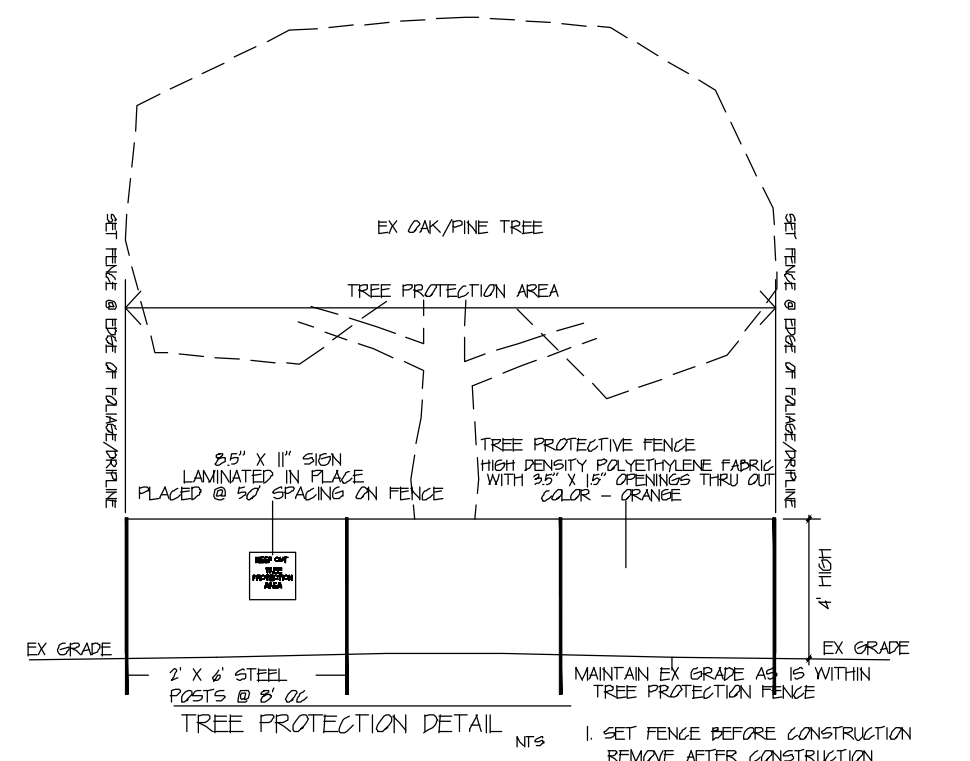
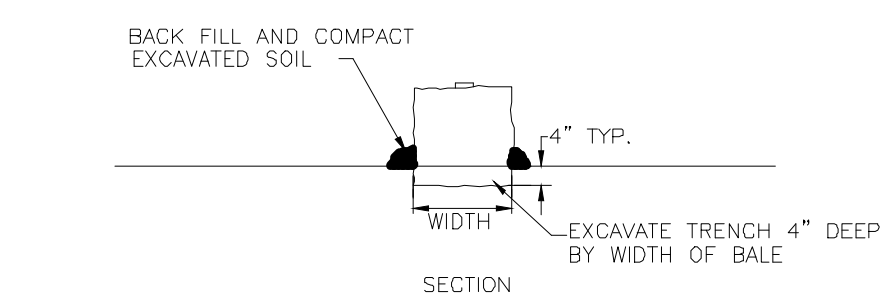
FILTER BARRIER
NOT TO SCALE



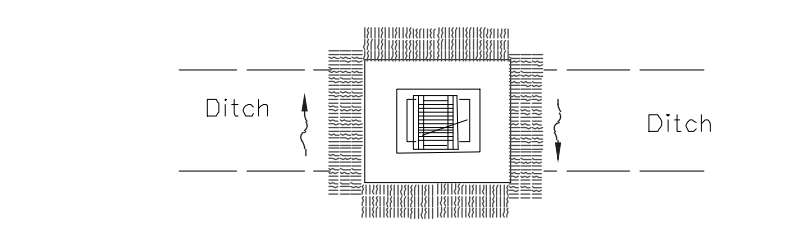
IN DRAINAGE WAY
NOT TO SCALE



STRAW BALE BARRIER
NOT TO SCALE



ACCESS POINT DETAIL - NOT IN ROW
NOT TO SCALE



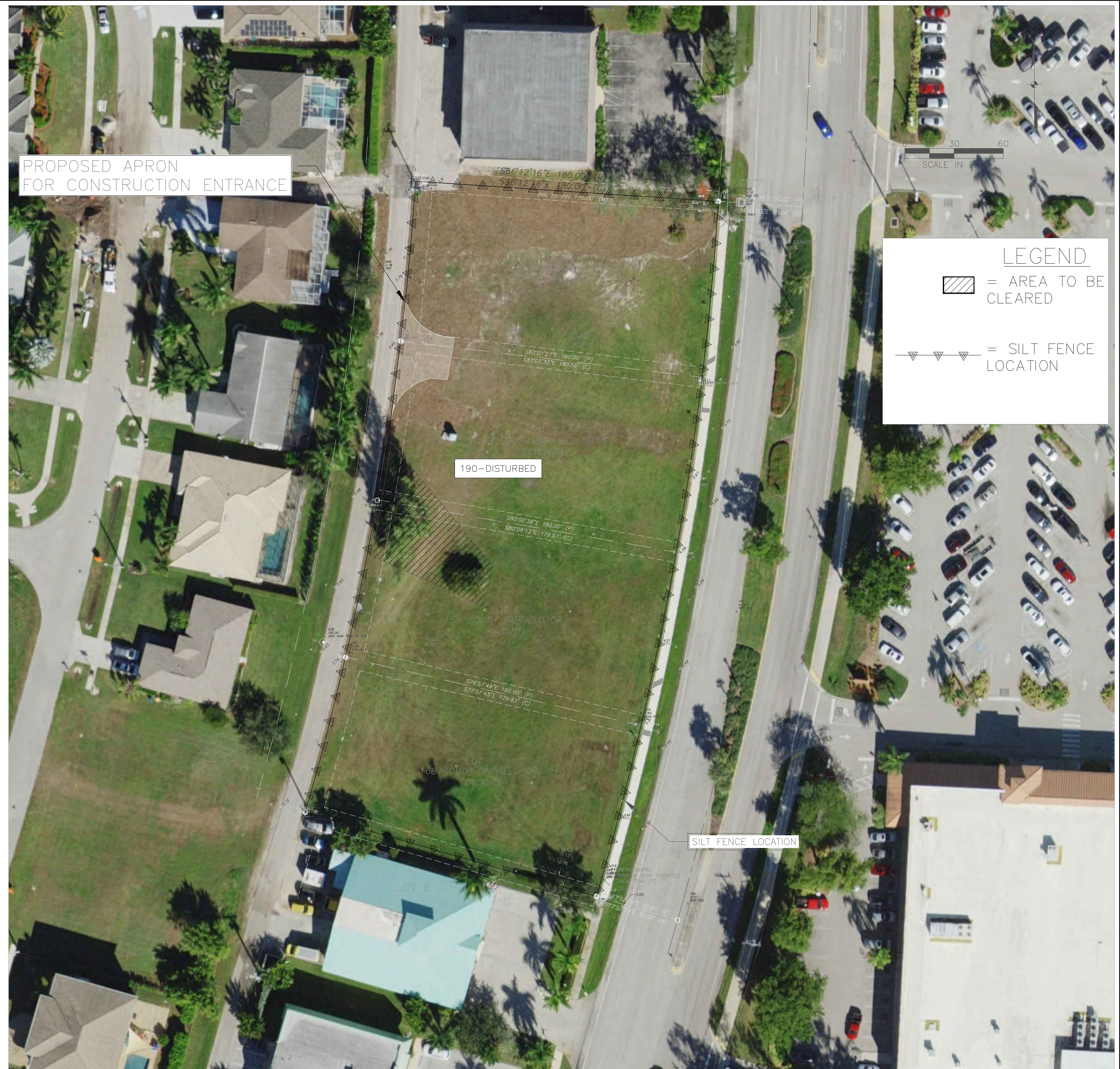
PROTECTION AROUND INLETS OR SIMILAR STRUCTURES

- CLEARING AND EROSION NOTES:**
- EXOTIC VEGETATION, AS DEFINED BY THE COLLIER COUNTY LAND DEVELOPMENT CODE (LDC), SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE RESPONSIBILITY OF THE PROPERTY OWNER.
 - THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZE THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EASEMENTS OF RECORD.
 - THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM.
 - BARRICADES AND SILT BARRIERS WILL REMAIN IN PLACE UNTIL FINAL GRADING AND SEEDING OR SODDING IS COMPLETE.
 - ALL PROPOSED INLETS SHALL BE PROTECTED WITH FILTER FABRIC DURING CONSTRUCTION.
 - ACCESS POINT SHALL BE A MINIMUM 6" THICK #1 FOOT STONE, AND IS TO EXTEND A MINIMUM OF 50' INTO THE PROJECT.
 - NO GRAVEL OR STONE SHALL BE WITHIN ANY ROW. A CONCRETE APRON SHALL BE PROVIDED FROM THE ASPHALT TO THE ROW LINE.
 - USE BARRICADES TO PROTECT VEGETATION TO REMAIN. SEE DETAIL.

PROPER PLACEMENT OF FILTER BARRIER IN DRAINAGE WAY
NOT TO SCALE

BOUNDARY & EXISTING CONDITIONS FROM: JOHNSON ENGINEERING, INC

AREA TO BE CLEARED = 0.1 ACRE



LEGEND

= AREA TO BE CLEARED

= SILT FENCE LOCATION

DESIGN	BAF	5			
DRAWN	AOM	3			
CHECKED	BAF	2			
D.C.		1			
	NO.	DATE	REVISION	APPO BY	

NOT VALID FOR CONSTRUCTION UNLESS SIGNED IN THIS BLOCK

Blair A. Foley, P.E., LLC
Civil Engineer / Development Consultant
120 Edgemere Way South - Naples, FL 34105
Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols00@aol.com

CLIENT: MARCO LUXE LLC

MARO LUXE
Collier County, Florida

EROSION CONTROL / AERIAL / FLUCCS CLEARING PLAN

CERT. OF AUTHORIZATION

Blair A. Foley, P.E.
No. 53758
STATE OF FLORIDA
Professional Engineer

PROJ. NO. _____
SCALE 1" = 30'
DATE 4/9/2024
SHT 2 OF 8

GENERAL NOTES

- ELEVATIONS ARE REFERENCED TO NAVD OF 1988.
- CURRENT ZONING FOR THIS PARCEL IS C-3.
- THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZE THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EXISTING EASEMENTS OF RECORD.
- HANDICAP PARKING AND ACCESSIBILITY SHALL BE IN ACCORDANCE WITH THE LATEST STATE OF FLORIDA REQUIREMENTS OR THE AMERICANS WITH DISABILITY ACT (ADA) WHICHEVER IS MORE STRINGENT.
- ALL DRIVEWAYS, LANDSCAPING, SIGNS, GRASS, ETC. SHALL BE RESTORED TO A CONDITION EQUIVALENT TO PRE CONSTRUCTION CONDITION UNLESS OTHERWISE APPROVED BY THE ENGINEER AND COUNTY.
- REQUESTS FOR INSPECTIONS SHALL BE PROVIDED TO CITY OF MARCO ISLAND STAFF AT LEAST FORTY EIGHT (48) HOURS PRIOR TO THE REQUESTED INSPECTION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE COUNTY AND ALL UTILITY COMPANIES IN THE AREA 48 HOURS MINIMUM PRIOR TO START OF CONSTRUCTION, AND SHALL HAVE ALL EXISTING UTILITY LINES (SPRINK, FL, WATER, CABLE, SANITARY, SEWER, IRRIGATION, FORCE MAIN & ANY OTHERS) LOCATED AND FLAGGED PRIOR TO ANY EXCAVATION.
- CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ENGINEER PRIOR TO CONSTRUCTION.
- THE CONTRACTOR IS REQUIRED TO OBTAIN WRITTEN APPROVAL FROM THE ENGINEER FOR ANY DEVIATIONS FROM THE APPROVED PLANS AND/OR SPECIFICATIONS.
- CONTRACTOR SHALL PROMPTLY REPORT ALL FIELD CHANGES TO THE ENGINEER.
- THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS, ALL EXISTING CONFLICTS TO DESIGN PLANS (I.E. POLES, BOXES, VALVES ETC.), AND ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY REQUIRED PLAN DEVIATIONS. CONTRACTOR IS SOLELY RESPONSIBLE FOR ALL DAMAGES AS A RESULT OF ALL CONFLICTS NOT SATISFACTORILY RESOLVED. ENGINEER IS NOT LIABLE FOR ANY CONFLICT SHOWN OR OMITTED FROM THIS PLAN OR SURVEY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC AND USAGE OF THE EXISTING STREETS ADJACENT TO THE PROJECT SITE. ALL TRAFFIC MAINTENANCE CONTROL SHALL BE IN ACCORDANCE WITH THE FLORIDA MANUAL OF TRAFFIC AND SAFE PRACTICES FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, AND UTILITY OPERATIONS.
- ALL DISTURBED PERVIOUS AREAS NOT LANDSCAPED SHALL BE SOODED AND/OR SEEDED AND MULCHED AS DIRECTED BY THE OWNER OR ENGINEER.
- DIMENSIONS WHICH REFERENCE CURBING ARE SHOWN TO FACE OF CURB UNLESS OTHERWISE NOTED.
- ALL CONSTRUCTION METHODS AND MATERIALS TO CITY OF MARCO ISLAND LAND DEVELOPMENT CODE AND F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION.
- SEE CLEARING PLAN FOR LIMITS OF CLEARING.
- THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE INFRASTRUCTURE, OPEN SPACES, & PAVED AREAS ON THEIR PROPERTY.
- FINAL RESTORATION UPON REMOVAL OF TEMPORARY CONSTRUCTION ENTRANCE: UPON COMPLETION OF THE PROJECT, THE TEMPORARY CONSTRUCTION ENTRANCE AND ASSOCIATED IMPROVEMENTS SHALL BE REMOVED AND THE RIGHT-OF-WAY, INCLUDING GROUND COVER SHALL BE RESTORED.

LANDSCAPING

- ALL EXOTIC VEGETATION AS DEFINED BY THE CITY OF MARCO ISLAND L.D.C. SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE RESPONSIBILITY OF THE OWNER.
- THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR THE MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM.
- CONTRACTOR SHALL REMOVE LIMEROCK WITHIN LANDSCAPE ISLANDS LOCATED WITHIN AND ADJACENT TO THE PARKING AREAS. THESE AREAS SHALL FILLED WITH CLEAN DIRT OR AS DIRECTED BY THE LANDSCAPE ARCHITECT.

PAVING, GRADING AND DRAINAGE NOTES:

- THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE SURFACE WATER MANAGEMENT SYSTEM AS OUTLINED BY THE DESIGN ENGINEER ON THESE DRAWINGS.
- ALL ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM (NAVD) 1988.
- MIN. FINISHED FLOOR ELEVATIONS SHALL BE AS SHOWN ON THE SITE PLAN.
- SITE IS LOCATED WITHIN THE FLOOD ZONE AE.
- HAY BALES, SILT SCREENS AND OTHER EROSION CONTROL MEASURES SHALL BE UTILIZED TO CONTAIN ALL DISTURBED AREAS IN ACCORDANCE WITH THE EROSION CONTROL DETAILS.
- SILT SCREENS TO BE INSTALLED AROUND PERIMETER OF PROPERTY AND/OR DISTURBED AREAS AND MAINTAINED DURING CONSTRUCTION.
- ALL PROPOSED ON-SITE PAVING SHALL BE ASPHALT, UNLESS OTHERWISE NOTED.
- WALKWAYS, DRIVEWAYS, DRIVEWAY WALKS SHALL BE CONCRETE OR PAVED BRICKS.
- A CITY OF MARCO ISLAND PERMIT TO PERFORM WORK AND/OR MAINTENANCE IN THE PUBLIC RIGHT OF WAY IS REQUIRED FOR WORK WITHIN S. BARFIELD DR. AND SILVER SPRAY WAY.
- THE FOLLOWING FOOT DETAIL INDEX SHEETS APPLY FOR ALL ROW ITEMS:
INDEX NO. 425-052 - DITCH BOTTOM DETAILS
INDEX NO. 522-002 - DETECTABLE WARNINGS / SIDEWALK RAMPS
INDEX NO. 522-003 - TURNOUTS
INDEX NO. 711-001 - SPECIAL MARKING AREAS.

UTILITY NOTES:

- ALL WATER DISTRIBUTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS.
- ALL SEWAGE COLLECTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS.
- ALL ON SITE WATER AND SEWER IS TO BE OWNED AND MAINTAINED BY THE OWNER, UNLESS OTHERWISE NOTED.
- THE LOCATION OF EXISTING UTILITIES HAS BEEN PREPARED FROM THE MOST RELIABLE INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT GUARANTEED, THEREFORE.
- THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE FIELD PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL UTILITY LINES AND SERVICES DAMAGED DURING CONSTRUCTION, INCLUDING IRRIGATION LINES AND SERVICES. THE APPROPRIATE UTILITY SHALL BE NOTIFIED OF ALL DAMAGED LINES PRIOR TO REPAIR. ALL NECESSARY REPAIRS SHALL BE PERFORMED IMMEDIATELY UPON DAMAGED OF LINE.
- ALL UTILITY CONNECTIONS SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE AGENCY HAVING JURISDICTION OF EACH INDIVIDUAL UTILITY LINE.
- THE UNDERGROUND CONTRACTOR SHALL MINIMIZE THE WORK AREA AND WIDTH OF TRENCHES TO AVOID DISTURBANCES OF NATURAL VEGETATION. SPOIL FROM TRENCHES SHALL BE PLACED ONLY ON PREVIOUSLY CLEARED AREAS OR AS DIRECTED BY THE ENGINEER OR COUNTY.
- THE CONTRACTOR SHALL NOT REMOVE OR DISTURB ANY TREES OR SHRUBS WITHOUT PRIOR APPROVAL FROM THE ENGINEER OR THE CITY.
- ALL CONDUITS REQUIRED FOR UTILITY INSTALLATIONS SHALL BE INSTALLED PRIOR TO THE STREET/PAVEMENT CONSTRUCTION.
- SITE LIGHTING SHALL BE PROVIDED, AS REQUIRED, BY ARCHITECT.
- ALL SEWER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF MARCO ISLAND PUBLIC UTILITIES DIVISION SPECIFICATIONS, LATEST EDITION.
- ALL WATER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH THE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS AND STANDARDS MANUAL, LATEST EDITION.
- CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE PROPERTY SIDE OF THE WATER SERVICE BACKFLOW PREVENTER.
- THE DEVELOPER'S CONTRACTOR TO PROVIDE BOLLARDS TO PROTECT FIRE HYDRANTS, AS NECESSARY.
- THE DEVELOPER'S CONTRACTOR TO PROVIDE A MINIMUM OF 10 FEET SEPARATION DISTANCE BETWEEN WATER SERVICES AND ELECTRICAL BOXES AND STREET LIGHTS.
- ALL COSTS AND EXPENSES OF ANY AND ALL REPAIRS, REPLACEMENTS, MAINTENANCE AND RESTORATIONS OF ABOVEGROUND IMPROVEMENTS PERMITTED WITHIN A CUE SHALL BE THE SOLE FINANCIAL RESPONSIBILITY OF THE GRANTOR, ITS SUCCESSORS OR ASSIGNS.

FIRE:

- A SEPARATE PERMIT IS REQUIRED PRIOR TO INSTALLATION OF ANY FIRE LINE.
- INSTALLATION OF ALL UNDERGROUND FIRE LINES SHALL COMPLY WITH THE 2022 EDITION OF NFPA 24.
- UNDERGROUND FIRE LINES SHALL BE INSTALLED BY AN APPROPRIATELY CERTIFIED SPRINKLER CONTRACTOR OR A TYPE V UNDERGROUND CONTRACTOR AS DEFINED BY AND OUTLINED F.S. 633.102(24).
- PIPING USED FOR FIRE PROTECTION SERVICE THAT IS RUN UNDER DRIVEWAYS SHALL BE BURIED AT A MINIMUM DEPTH OF 36".

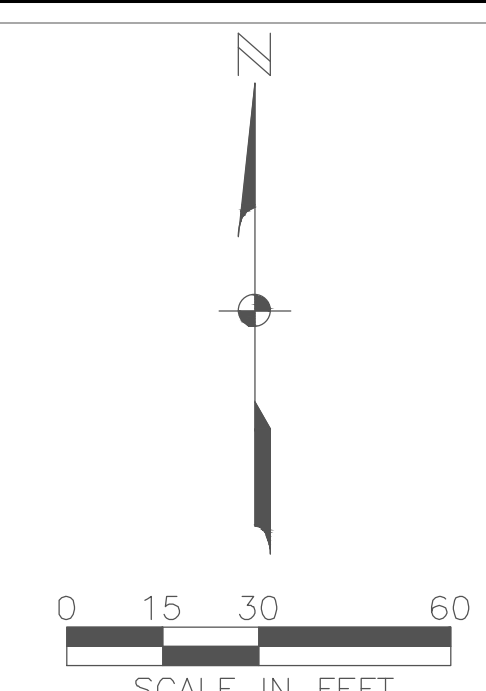
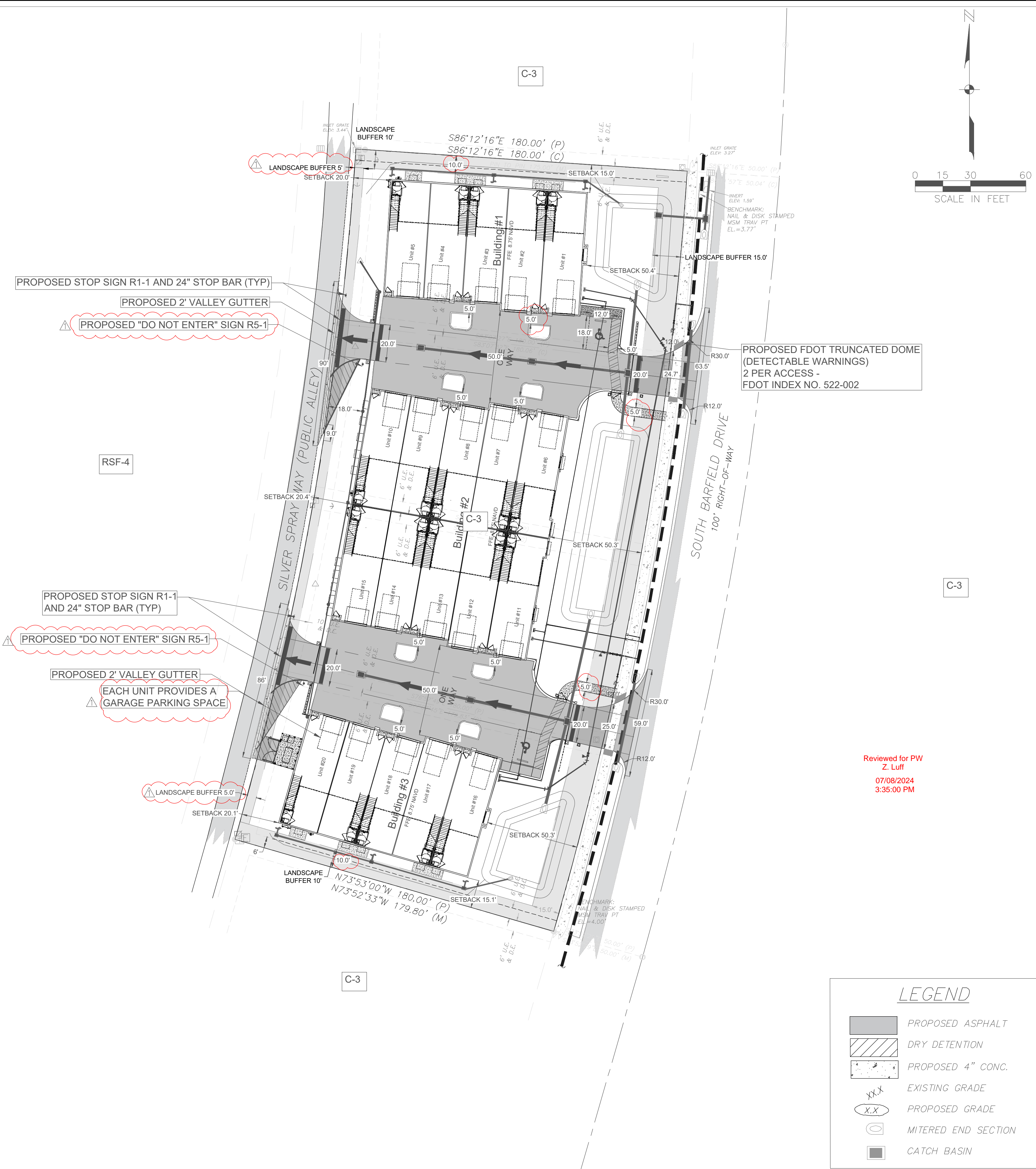
SITE SUMMARY TABLE					
TOTAL SITE			1.64	AC	
PROPOSED SITE SUMMARY					
BUILDING FOOTPRINT	27,900	SF	0.64	AC	39.1%
SIDEWALK/CONCRETE	1,780	SF	0.04	AC	2.5%
PAVEMENT (VUA)	18,900	SF	0.43	AC	26.5%
IMPERVIOUS SUBTOTAL	48,580	SF	1.12	AC	68.0%
OPEN SPACE:					
GREEN SPACE	16,493	SF	0.38	AC	23.1%
DRY DETENTION	6,365	SF	0.15	AC	8.9%
PERVIOUS SUBTOTAL	22,858	SF	0.52	AC	32.0%
TOTAL	71,438	SF	1.64	AC	100%

SETBACK TABLE (CITY OF MARCO ISLAND)			
PROPOSED BUILDING	PROVIDED	REQUIRED	
HEIGHT	25' 5"	50'	
SETBACK FROM PROPERTY LINE-BUILDING 1			
FRONT	E = 50.3'	50'	
REAR	W = 20.1'	15'	
SIDE	N = N/A	15'	
SIDE	S = 15.1'	15'	
SETBACK FROM PROPERTY LINE-BUILDING 2			
FRONT	E = 50.3'	50'	
REAR	W = 20.4'	15'	
SIDE	N = N/A	15'	
SIDE	S = N/A	15'	
SETBACK FROM PROPERTY LINE-BUILDING 3			
FRONT	E = 50.4'	50'	
REAR	W = 20.0'	15'	
SIDE	N = 15.0'	15'	
SIDE	S = N/A	15'	
BUILDING SEPARATION			
BUILDINGS 1&2	50'		
BUILDINGS 2&3	50'		

ZONING SUMMARY	LAND USE
SITE	C-3
NORTH	C-3
SOUTH	C-3
EAST	ROW S. BARFIELD DR.
EAST OF ROW	C-3
WEST	RW SILVER SPRAY WAY
WEST OF ROW	RSF-4

PARKING TABLE			
USE: STORAGE FACILITY: SELF-SERVICE			
	RATE	SF	TOTAL
PARKING REQUIRED: PROPOSED BLDG	1 per 20,000 SF of storage buildings	27,809	1.4
	Plus 1 per 50 vehicle	20	0.4
	Minimum of 4.		
TOTAL REQUIRED			4
PARKING PROVIDED	1 garage space within each unit		20
HANDICAP REQUIRED			1
HANDICAP PROVIDED			2
LOADING REQUIRED	74' X 10' FOR SELF-STORAGE		2
LOADING PROVIDED			2

NOTE: DRY FLOOD PROOFING IS PROPOSED TO 11.17' NAVD



Reviewed for PW
Z. Luff
07/08/2024
3:35:00 PM

LEGEND	
	PROPOSED ASPHALT
	DRY DETENTION
	PROPOSED 4\"/>

DESIGN	BAF	5			
		4			
DRAWN	AOM	3			
		2			
CHECKED	BAF	1	6/27/24	City of Marco Comment Letter dated 6/11/24	BAF
D.C.	NO.	DATE	REVISION		APPD BY

Blair A. Foley, P.E., LLC
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CLIENT: MARCO LUXE LLC
MARCO LUXE
 Collier County, Florida

SITE PLAN
 SCALE 1" = 30'
 DATE 4/9/2024
 SHEET 3 OF 8

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- NOTE: ALL BLDG. PERIMETER WALKS SHALL BE CONCRETE OR PAVED BRICKS.
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 - INDEX NO. 425-052 - DITCH BOTTOM INLETS.
 - INDEX NO. 522-000 - TURNOUTS / SIDEWALK RAMPS
 - INDEX NO. 522-003 - TURNOUTS
 - INDEX NO. 711-001 - SPECIAL MARKING AREAS.

UTILITY NOTES:

- ALL WATER DISTRIBUTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH WATER AND SEWER DEPT., MANUAL OF STANDARDS AND SPECIFICATIONS, LATEST EDITION.
- ALL SEWAGE COLLECTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH WATER AND SEWER DEPT., MANUAL OF STANDARDS AND SPECIFICATIONS, LATEST EDITION.
- ALL ON SITE WATER AND SEWER IS TO BE OWNED AND MAINTAINED BY THE OWNER, UNLESS OTHERWISE NOTED.
- THE LOCATION OF EXISTING UTILITIES HAS BEEN PREPARED FROM THE MOST RELIABLE INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT GUARANTEED, THEREFORE THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE FIELD PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL UTILITY LINES AND SERVICES DAMAGED DURING CONSTRUCTION, INCLUDING IRRIGATION LINES AND SERVICES.
- APPROPRIATE UTILITY SHALL BE NOTIFIED OF ALL DAMAGED LINES PRIOR TO REPAIR. ALL NECESSARY REPAIRS SHALL BE PERFORMED IMMEDIATELY UPON DAMAGED OF LINE.
- ALL UTILITY CONNECTIONS SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE AGENCY HAVING JURISDICTION OF EACH INDIVIDUAL UTILITY LINE.
- THE UNDERGROUND CONTRACTOR SHALL MINIMIZE THE WORK AREA AND WIDTH OF TRENCHES TO AVOID DISTURBANCES OF NATURAL VEGETATION. SPILL FROM TRENCHES SHALL BE PLACED ONLY ON PREVIOUSLY CLEARED AREAS OR AS DIRECTED BY THE ENGINEER OR COUNTY.
- THE CONTRACTOR SHALL NOT REMOVE OR DISTURB ANY TREES OR SHRUBS WITHOUT PRIOR APPROVAL FROM THE ENGINEER OR THE CITY.
- ALL CONDUITS REQUIRED FOR UTILITY INSTALLATIONS SHALL BE INSTALLED PRIOR TO THE STREET/PAVEMENT CONSTRUCTION.
- SITE LIGHTING SHALL BE PROVIDED, AS REQUIRED, BY ARCHITECT.
- ALL SEWER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF MARCO ISLAND WATER AND SEWER DEPT., MANUAL OF STANDARDS AND SPECIFICATIONS, LATEST EDITION.
- ALL WATER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH THE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS AND STANDARDS MANUAL, LATEST EDITION.
- CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE PROPERTY SIDE OF THE WATER SERVICE BACKFLOW PREVENTER.
- THE DEVELOPER'S CONTRACTOR TO PROVIDE BOLLARDS TO PROTECT FIRE HYDRANTS, AS NECESSARY.
- THE DEVELOPER'S CONTRACTOR TO PROVIDE A MINIMUM OF 10 FEET SEPARATION DISTANCE BETWEEN WATER SERVICES AND ELECTRICAL BOXES AND STREET LIGHTS.
- ALL COSTS AND EXPENSES OF ANY AND ALL REPAIRS, REPLACEMENTS, MAINTENANCE AND RESTORATIONS OF ABOVEGROUND IMPROVEMENTS PERMITTED WITHIN A CUE SHALL BE THE SOLE FINANCIAL RESPONSIBILITY OF THE GRANTOR, ITS SUCCESSORS OR ASSIGNS.

FIRE:

- A SEPARATE PERMIT IS REQUIRED PRIOR TO INSTALLATION OF ANY FIRE LINE.
- INSTALLATION OF ALL UNDERGROUND FIRE LINES SHALL COMPLY WITH THE 2022 EDITION OF NFPA 24.
- UNDERGROUND FIRE LINES SHALL BE INSTALLED BY AN APPROPRIATELY CERTIFIED SPRINKLER CONTRACTOR OR A TYPE V UNDERGROUND CONTRACTOR AS DEFINED BY AND OUTLINED F.S. 633.102(24)
- PIPING USED FOR FIRE PROTECTION SERVICE THAT IS RUN UNDER DRIVEWAYS SHALL BE BURIED AT A MINIMUM DEPTH OF 36".

NOTE: FIRE HYDRANT MUST BE CLOW MEDALLION

FIRE METER: 6" NEPTUNE C&I MACH 10 R900, 2.0 TO 2,000 US GPM, FIRE FLOW 1,800 US GPM

FIRE FLOW REQUIREMENTS

FBC TYPE IIB, SPRINKLED	FLOW REQUIREMENT	75% RED.	MIN. REQUIREMENT
Building 1: 6,909 SF	1,750 GPM 2 HR	438 GPM	1,000 GPM
Building 2: 13,991 SF	2,500 GPM 2 HR	625 GPM	1,000 GPM
Building 3: 6,909 SF	1,750 GPM 2 HR	438 GPM	1,000 GPM

According to NFPA Table 18.4.5.2.1 Fire Flow Requirement, 75% Reduction Allowed for Sprinkled Buildings OR 1000 GPM Min = **1,000 GPM**

NOTE: SEE CITY OF MARCO ISLAND APPROVED PRODUCTS LIST, APPENDIX I, CITY OF MARCO ISLAND WATER & SEWER DEPARTMENT, MANUAL OF STANDARD AND SPECIFICATION

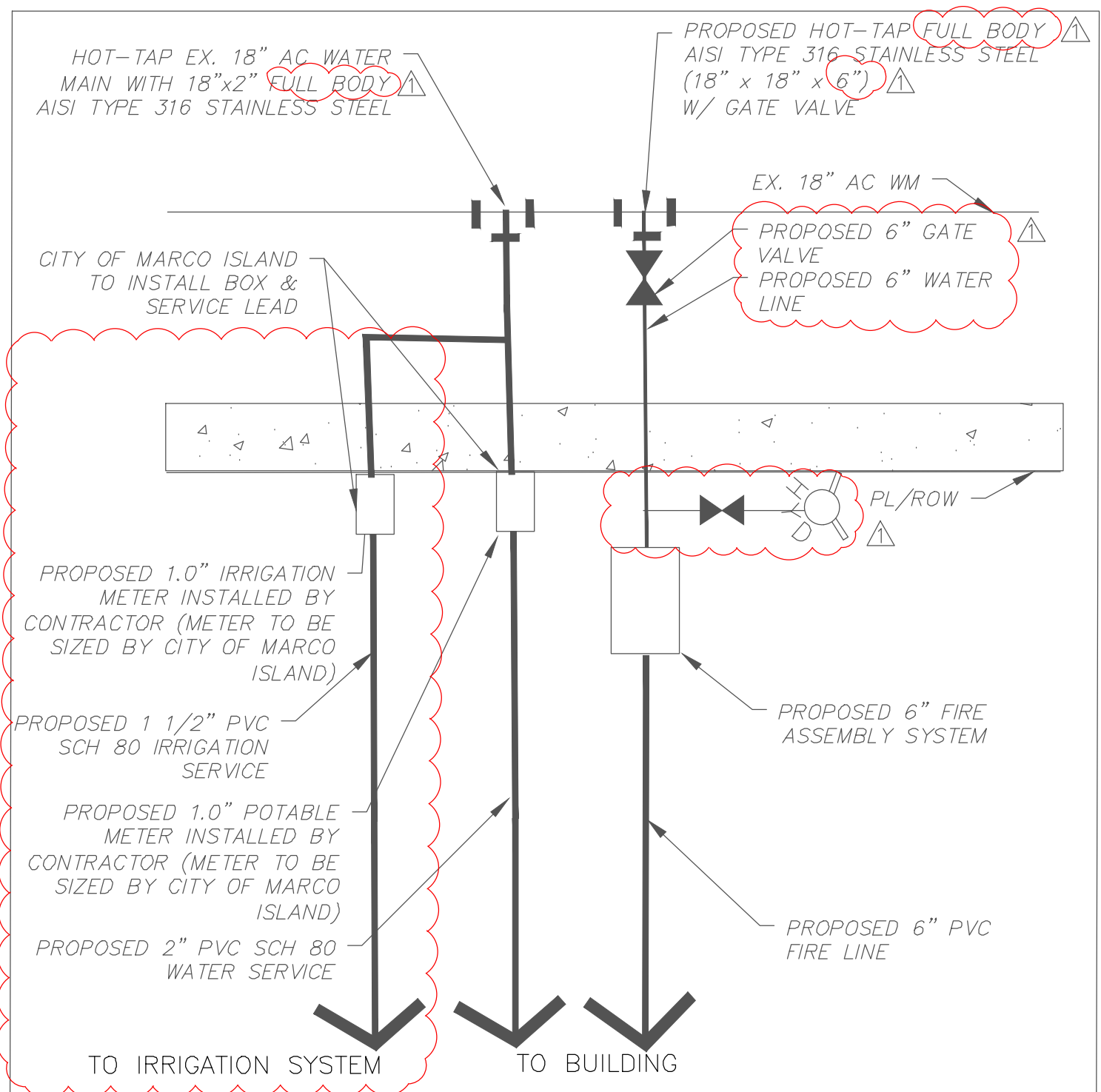
NOTE: POTABLE AND IRRIGATION FLOW METER MANUFACTURER MUST BE "NEPTUNE" R900VI RADIO READ AND RECORD IN GALLONS.

NOTE: BACK FLOW PREVENTION DEVICE MANUFACTURERS MAY BE: AMES/WATTS 400B/919, WATTS 909-009 SERIES, WILKENS 975,

FIRE BFP MAY BE AMES/WATTS C300/M300, WATTS 709, WILKENS 350 OR 450 N-SHAPES. (ALL MUST BE RPZ)

SEE CMI MANUAL OF STANDARDS APPENDIX I-6

NOTE: EXISTING AC WATER MAIN IS +/- 18" OUTSIDE DIAMETER. THE OD IS KNOWN TO VARY. EXCAVATE AND MEASURE THE OD AT THE LOCATION OF THE PROPOSED CONNECTION TO CONFIRM THE CORRECT SIZE FOR THE HOT TAP PARTS.

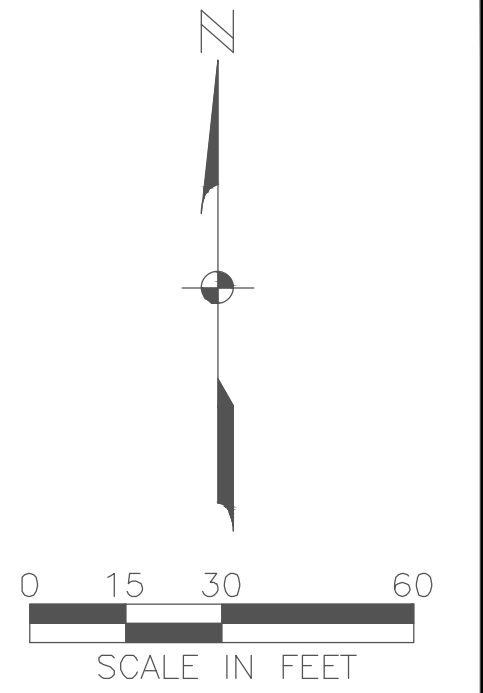
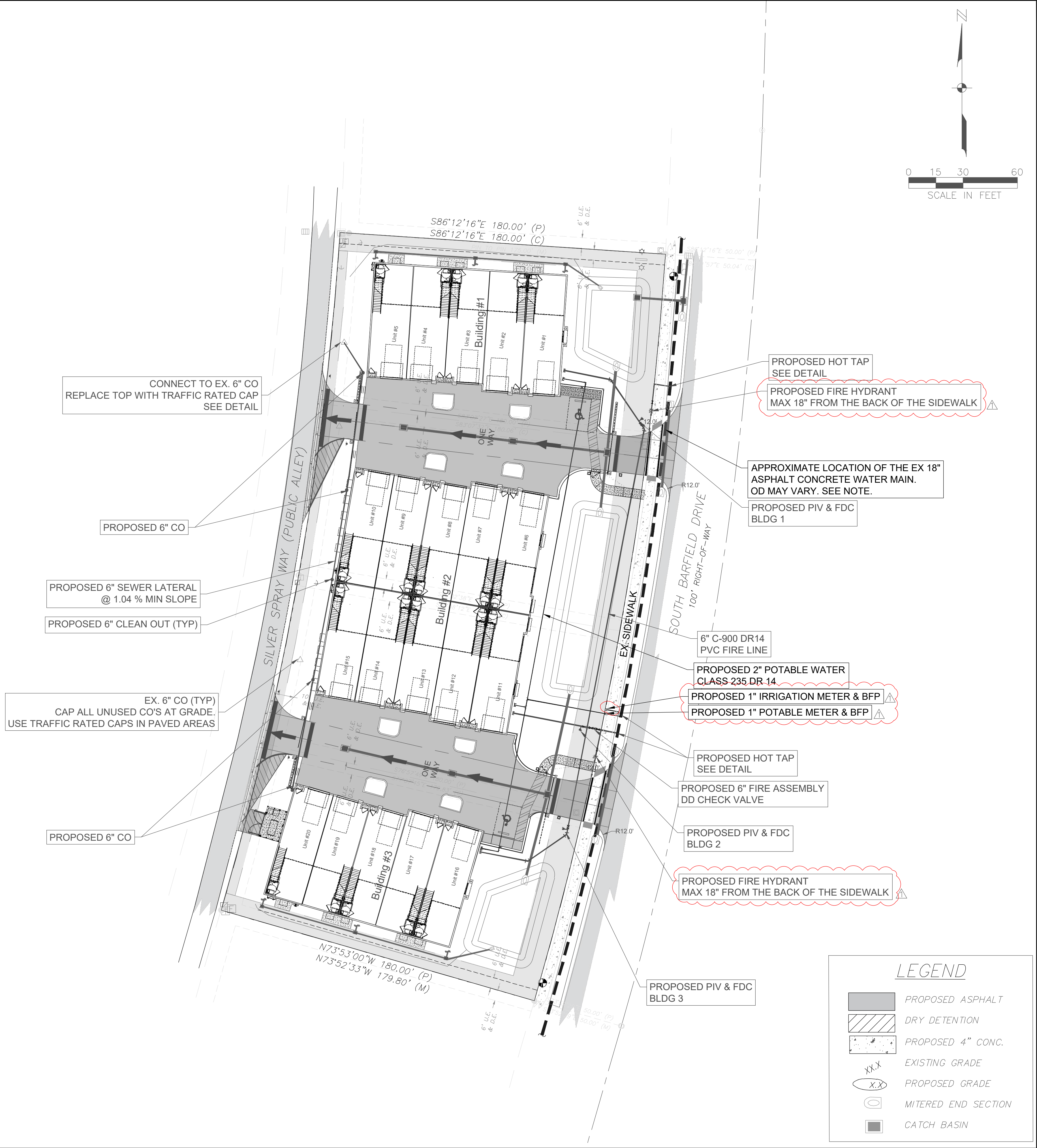


NOTE:
 1. POINT OF SERVICE IS GATE VALVE AT THE ROW LINE
 2. THE WATER MAIN IS ASBESTOS CONCRETE, ALL PARTS FOR THIS PIPE ARE SPECIAL MADE UPON REQUEST. IT IS ADVISED TO OBTAIN O.D. OF PIPE THEN PLACE ORDER THEN MAKE SURE THE SAME SPOT IS EXCAVATED OR OD MAY CHANGE DURING TRANSITION TO NEXT PIPE JOINT.

HOT-TAP WATER / METER CONNECTION DETAIL NTS

NOTE: LATERALS SHALL HAVE A MINIMUM DEPTH OF 30" AND A MAXIMUM DEPTH OF 48" BELOW FINISHED GRADE. IN LOCATIONS WHERE A MINIMUM DEPTH OF 30" CANNOT BE PROVIDED, LATERALS SHALL BE C900, DR 14 PVC PIPE. AT NO TIME SHALL THE DEPTH OF A LATERAL BE LESS THAN 24".

ADDITIONALLY, SEWER LATERALS SHOULD HAVE 30" (MIN.) TO 48" (MAX.) COVER AT THE CLEANOUT PER SECTION 3.1.4 OF OUR DESIGN CRITERIA AND AS SHOWN IN DETAIL WW-10.



LEGEND

	PROPOSED ASPHALT
	DRY DETENTION

DESIGN	DATE	BY	APPD BY
BAF	5		
BAF	4		
AOM	3		
BAF	2		
BAF	1	6/27/24	City of Marco Comment Letter dated 6/11/24

Blair A. Foley, P.E., LLC
 Civil Engineer / Development Consultant
 120 Edgemere Way South - Naples, FL 34105
 Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols00@aol.com

CLIENT: MARCO LUXE LLC
MARCO LUXE
 Collier County, Florida

UTILITY PLAN

PROJ. NO. _____
 SCALE 1" = 30'
 DATE 4/9/2024
 SHEET 4 OF 8

GENERAL NOTES

- ELEVATIONS ARE REFERENCED TO NAVD OF 1988.
- CURRENT ZONING FOR THIS PARCEL IS C-3.
- THE REVIEW AND APPROVAL OF IMPROVEMENT PLANS DOES NOT AUTHORIZED THE CONSTRUCTION OF REQUIRED IMPROVEMENTS WHICH ARE INCONSISTENT WITH EXISTING EASEMENTS OF RECORD.
- HANDICAP PARKING AND ACCESSIBILITY SHALL BE IN ACCORDANCE WITH THE LATEST STATE OF FLORIDA REQUIREMENTS OR THE AMERICANS WITH DISABILITY ACT (ADA) WHICHEVER IS MORE STRINGENT.
- ALL DRIVEWAYS, LANDSCAPING, SIGNS, GRASS, ETC. SHALL BE RESTORED TO A CONDITION EQUIVALENT TO PRE CONSTRUCTION CONDITION UNLESS OTHERWISE APPROVED BY THE ENGINEER AND COUNTY.
- REQUESTS FOR INSPECTIONS SHALL BE PROVIDED TO CITY OF MARCO ISLAND STAFF AT LEAST FORTY EIGHT (48) HOURS PRIOR TO THE REQUESTED INSPECTION.
- THE CONTRACTOR SHALL NOTIFY THE ENGINEER, THE COUNTY AND ALL UTILITY COMPANIES IN THE AREA 48 HOURS MINIMUM PRIOR TO START OF CONSTRUCTION, AND SHALL HAVE ALL EXISTING UTILITY LINES (SPRINT, FPL, WATER, CABLE, SANITARY, SEWER, IRRIGATION, FORCE MAIN & ANY OTHERS) LOCATED AND FLAGGED PRIOR TO ANY EXCAVATION.
- CONTRACTOR TO SUBMIT SHOP DRAWINGS TO ENGINEER PRIOR TO CONSTRUCTION.
- THE CONTRACTOR IS REQUIRED TO OBTAIN WRITTEN APPROVAL FROM THE ENGINEER FOR ANY DEVIATIONS FROM THE APPROVED PLANS AND/OR SPECIFICATIONS.
- CONTRACTOR SHALL PROMPTLY REPORT ALL FIELD CHANGES TO THE ENGINEER.
- THE CONTRACTOR SHALL FIELD VERIFY ALL DIMENSIONS, ALL EXISTING CONFLICTS TO DESIGN PLANS (I.E. POLES, BOXES, VALVES ETC.) AND ELEVATIONS PRIOR TO COMMENCEMENT OF CONSTRUCTION AND NOTIFY THE ENGINEER IMMEDIATELY OF ANY REQUIRED PLAN DEVIATIONS. CONTRACTOR IS SOLELY RESPONSIBLE FOR ALL DAMAGES AS A RESULT OF ALL CONFLICTS NOT SATISFACTORILY RESOLVED. ENGINEER IS NOT LIABLE FOR ANY CONFLICT SHOWN OR OMITTED FROM THIS PLAN OR SURVEY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC AND USAGE OF THE EXISTING STREETS ADJACENT TO THE PROJECT SITE. ALL TRAFFIC MAINTENANCE CONTROL SHALL BE IN ACCORDANCE WITH THE FLORIDA MANUAL OF TRAFFIC AND SAFE PRACTICES FOR STREET AND HIGHWAY CONSTRUCTION, MAINTENANCE, AND UTILITY OPERATIONS.
- ALL DISTURBED PERVIOUS AREAS NOT LANDSCAPED SHALL BE SODDED AND/OR SEEDED AND MULCHED AS DIRECTED BY THE OWNER OR ENGINEER.
- DIMENSIONS WHICH REFERENCE CURBING ARE SHOWN TO FACE OF CURB UNLESS OTHERWISE NOTED.
- ALL CONSTRUCTION METHODS AND MATERIALS TO CITY OF MARCO ISLAND LAND DEVELOPMENT CODE AND F.D.O.T. STANDARD SPECIFICATIONS FOR ROAD CONSTRUCTION.
- SEE CLEARING PLAN FOR LIMITS OF CLEARING.
- THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE INFRASTRUCTURE, OPEN SPACES, & PAVED AREAS ON THEIR PROPERTY.
- FINAL RESTORATION UPON REMOVAL OF TEMPORARY CONSTRUCTION ENTRANCE. UPON COMPLETION OF THE PROJECT, THE TEMPORARY CONSTRUCTION ENTRANCE AND ASSOCIATED IMPROVEMENTS SHALL BE REMOVED AND THE RIGHT-OF-WAY, INCLUDING GROUND COVER SHALL BE RESTORED.

LANDSCAPING

- ALL EXOTIC VEGETATION AS DEFINED BY THE CITY OF MARCO ISLAND L.D.C. SHALL BE REMOVED FROM THE SITE AND SUBSEQUENT ANNUAL EXOTIC REMOVAL (IN PERPETUITY) SHALL BE THE RESPONSIBILITY OF THE OWNER.
- THE PROPERTY OWNER IS RESPONSIBLE FOR REPLACEMENT OF ALL DEAD LANDSCAPE MATERIAL AND FOR THE MAINTENANCE OF THE REQUIRED IRRIGATION SYSTEM.
- CONTRACTOR SHALL REMOVE LIMBROCK WITHIN LANDSCAPE AREAS LOCATED WITHIN AND ADJACENT TO THE PARKING AREAS. THESE AREAS SHALL FILLED WITH CLEAN DIRT OR AS DIRECTED BY THE LANDSCAPE ARCHITECT.

PAVING, GRADING AND DRAINAGE NOTES:

- THE PROPERTY OWNER IS RESPONSIBLE FOR THE PERPETUAL MAINTENANCE OF ALL FEATURES OF THE SURFACE WATER MANAGEMENT SYSTEM AS OUTLINED BY THE DESIGN ENGINEER ON THESE DRAWINGS.
- ALL ELEVATIONS ARE BASED ON NORTH AMERICAN VERTICAL DATUM (NAVD) 1988.
- MIN. FINISHED FLOOR ELEVATIONS SHALL BE AS SHOWN ON THE SITE PLAN.
- THE SITE IS LOCATED WITHIN THE FLOOD ZONE AE.
- HAY BALES, SILT SCREENS AND OTHER EROSION CONTROL MEASURES SHALL BE UTILIZED TO CONTAIN ALL DISTURBED AREAS IN ACCORDANCE WITH THE EROSION CONTROL DETAILS.
- SILT SCREENS TO BE INSTALLED AROUND PERIMETER OF PROPERTY AND/OR DISTURBED AREAS AND MAINTAINED DURING CONSTRUCTION.
- ALL PROPOSED ON-SITE PAVING SHALL BE ASPHALT, UNLESS OTHERWISE NOTED.
- NOTE: ALL BLDG. PERIMETER WALKS SHALL BE CONCRETE OR PAVER BRICKS.
- A CITY OF MARCO ISLAND PERMIT TO PERFORM WORK AND/ OR MAINTENANCE IN THE PUBLIC RIGHT OF WAY IS REQUIRED FOR WORK WITHIN S. BARFIELD DR. AND SILVER SPRAY WAY.
- THE FOLLOWING "DOT" DETAIL MEASUREMENTS APPLY FOR ALL "ROW" ITEMS:
INDEX NO. 425-052 = DITCH BOTTOM INLETS,
INDEX NO. 522-002 = DETECTABLE WARNINGS / SIDEWALK RAMPS
INDEX NO. 522-003 = TURNOUTS
INDEX NO. 711-001 = SPECIAL MARKING AREAS.

UTILITY NOTES:

- ALL WATER DISTRIBUTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS.
- ALL SEWAGE COLLECTION SYSTEM CONSTRUCTION MATERIALS AND METHODS SHALL BE IN ACCORDANCE WITH APPLICABLE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS.
- ALL ON-SITE WATER AND SEWER IS TO BE OWNED AND MAINTAINED BY THE OWNER, UNLESS OTHERWISE NOTED.
- THE LOCATION OF EXISTING UTILITIES HAS BEEN PREPARED FROM THE MOST RELIABLE INFORMATION AVAILABLE TO THE ENGINEER. THE INFORMATION IS NOT GUARANTEED, THEREFORE.
- THE CONTRACTOR SHALL VERIFY THE LOCATION AND ELEVATION OF ALL UTILITIES IN THE FIELD PRIOR TO THE START OF ANY CONSTRUCTION ACTIVITIES.
- THE CONTRACTOR IS RESPONSIBLE FOR REPAIRING ALL UTILITY LINES AND SERVICES DAMAGED DURING CONSTRUCTION, INCLUDING IRRIGATION LINES AND SERVICES. THE APPROPRIATE UTILITY SHALL BE NOTIFIED OF ALL DAMAGED LINES PRIOR TO REPAIR. ALL NECESSARY REPAIRS SHALL BE PERFORMED IMMEDIATELY UPON DAMAGED OR LINE.
- ALL UTILITY CONNECTIONS SHALL CONFORM TO THE STANDARDS AND REQUIREMENTS OF THE AGENCY HAVING JURISDICTION OF EACH INDIVIDUAL UTILITY LINE.
- THE UNDERGROUND CONTRACTOR SHALL MINIMIZE THE WORK AREA AND WIDTH OF TRENCHES TO AVOID DISTURBANCES OF NATURAL VEGETATION. SPOIL FROM TRENCHES SHALL BE PLACED ONLY ON PREVIOUSLY CLEARED AREAS OR AS DIRECTED BY THE ENGINEER OR COUNTY.
- THE CONTRACTOR SHALL NOT REMOVE OR DISTURB ANY TREES OR SHRUBS WITHOUT PRIOR APPROVAL FROM THE ENGINEER OR THE CITY.
- ALL CONDUITS REQUIRED FOR UTILITY INSTALLATIONS SHALL BE INSTALLED PRIOR TO THE STREET/PAVEMENT CONSTRUCTION.
- SITE LIGHTING SHALL BE PROVIDED, AS REQUIRED, BY ARCHITECT.
- ALL SEWER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH CITY OF MARCO ISLAND PUBLIC UTILITIES DIVISION SPECIFICATIONS, LATEST EDITION.
- ALL WATER UTILITY CONSTRUCTION TO BE IN ACCORDANCE WITH THE CITY OF MARCO ISLAND UTILITIES SPECIFICATIONS AND STANDARDS MANUAL, LATEST EDITION.
- CITY OF MARCO ISLAND OWNERSHIP & MAINTENANCE RESPONSIBILITIES STOP AT THE PROPERTY SIDE OF THE WATER SERVICE BACKFLOW PREVENTER.
- THE DEVELOPER'S CONTRACTOR TO PROVIDE BOLLARDS TO PROTECT FIRE HYDRANTS, AS NECESSARY.
- THE DEVELOPER'S CONTRACTOR TO PROVIDE A MINIMUM OF 10 FEET SEPARATION DISTANCE BETWEEN WATER SERVICES AND ELECTRICAL BOXES AND STREET LIGHTS.
- ALL COSTS AND EXPENSES OF ANY AND ALL REPAIRS, REPLACEMENTS, MAINTENANCE AND RESTORATIONS OF ABOVEGROUND IMPROVEMENTS PERMITTED WITHIN A CUE SHALL BE THE SOLE FINANCIAL RESPONSIBILITY OF THE GRANTEE, ITS SUCCESSORS OR ASSIGNS.

FIRE:

- A SEPARATE PERMIT IS REQUIRED PRIOR TO INSTALLATION OF ANY FIRE LINE.
- INSTALLATION OF ALL UNDERGROUND FIRE LINES SHALL COMPLY WITH THE 2022 EDITION OF NFPA 24.
- UNDERGROUND FIRE LINES SHALL BE INSTALLED BY AN APPROPRIATELY CERTIFIED SPRINKLER CONTRACTOR OR A TYPE V UNDERGROUND CONTRACTOR AS DEFINED BY AND OUTLINED F.S. 633.102(24).
- PIPING USED FOR FIRE PROTECTION SERVICE THAT IS RUN UNDER DRIVEWAYS SHALL BE BURIED AT A MINIMUM DEPTH OF 36".

SITE SUMMARY TABLE				
TOTAL SITE			1.64	AC
PROPOSED SITE SUMMARY				
BUILDING FOOTPRINT	27,900	SF	0.64	AC 39.1%
SIDEWALK/CONCRETE	1,780	SF	0.04	AC 2.5%
PAVEMENT (VUA)	18,900	SF	0.43	AC 26.5%
IMPERVIOUS SUBTOTAL	48,580	SF	1.12	AC 68.0%
OPEN SPACE:				
GREEN SPACE	16,493	SF	0.38	AC 23.1%
DRY DETENTION	6,365	SF	0.15	AC 8.9%
PERVIOUS SUBTOTAL	22,858	SF	0.52	AC 32.0%
TOTAL	71,438	SF	1.64	AC 100%

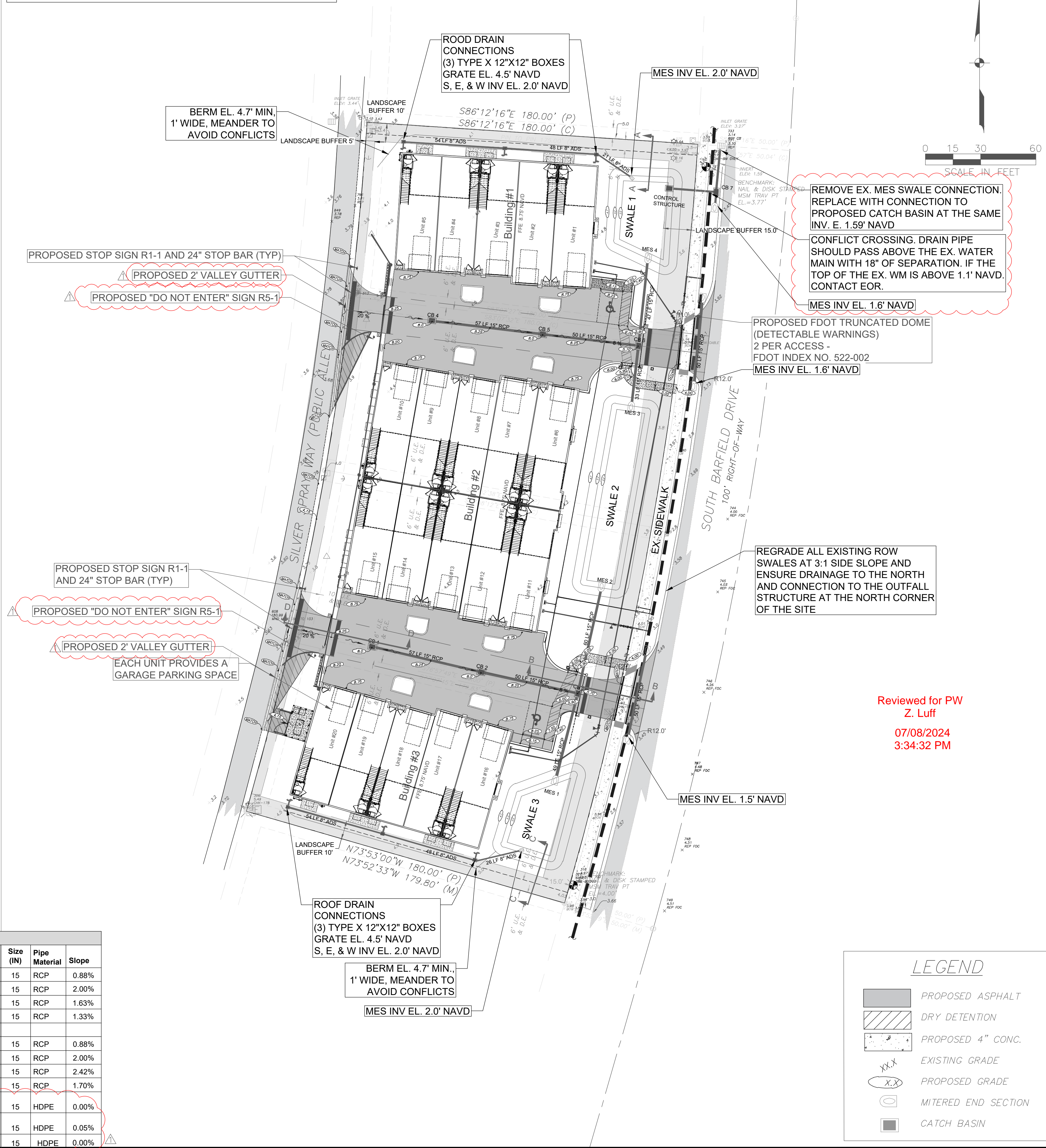
WATER MANAGEMENT SUMMARY	
SFWM Permit No. (N/A, no existing ERP)	
NGVD - 1.31' = NAVD	
10-YEAR, 1-DAY RAINFALL (in)	7.2
25-YEAR, 3-DAY RAINFALL (in)	12.50
100-YEAR, 3-DAY RAINFALL (in)	16.00
PEAK 10-YEAR, 1-DAY STORM STAGE (FT-NAVD)	4.15
PEAK 25-YEAR, 3-DAY STORM STAGE (FT-NAVD)	4.61
PEAK 100-YEAR, 3-DAY ZERO DISCHARGE STORM STAGE (FT-NAVD)	7.12
FEMA BFE (FT-NAVD) ZONE AE	10.00
PROPOSED DESIGN CONDITIONS:	
FFE (FT NAVD)	8.75
FLOOD PROOFING (FT NAVD)	11.17
MIN PARKING LOT EL. (FT-NAVD)	4.15
MIN BERM (FT-NAVD)	4.70
MIN ROAD (FT-NAVD)	5.70
ON-SITE CONTROL ELEVATION (FT-NAVD)	1.20

WATER QUALITY DETENTION VOLUME REQUIRED			
Basin Area =	1.64	ac	
First one (1) inch of runoff over basin:			
	1" * 1.64 ac x 1/12" =	0.14	ac ft
2.5 Inches Times the Percentage of Imperviousness:			
Site Area for Water Quality Treatment = Basin Area - Roof =	1.00	ac	
Impervious Area for W.Q. Treatment = Site Area for W.Q. - Total Pervious Area =	0.47	ac	
Percent Impervious = Impervious Area/Site Area =	47.50	%	
Volume to be Treated = 2.5" x Percent Impervious x Basin Area =	0.16	ac ft	
2.5 Inches Times the Percentage of Imperviousness Governs:			
25% reduction for using Dry Detention =	0.12	ac ft	
150% increase for discharge to Impaired Waters =	0.18	ac ft	

WATER QUALITY VOLUME PROVIDED			
Pipe Storage			
251 LF	8 in	Volume =	88 CF
403 LF	15 in	Volume =	495 CF
		Total Pipe Volume =	582 CF
Swale Storage			
		Swale 1 Volume =	3360 CF
		Swale 2 Volume =	5820 CF
		Swale 3 Volume =	3450 CF
		Swale Volume from 2.2' to 4.7' =	12,630 CF
		Total Volume =	13,212 CF
			0.30 AC FT
Water Quality Detention occurs at		3.1'	FT

DRAINAGE TABLE											
From	Structure Description	Invert El. (NAVD)	Grate El. (NAVD)	To	Structure Description	Invert El. (NAVD)	Grate El. (NAVD)	Length (FT)	Size (IN)	Pipe Material	Slope
CB 1	TYPE "C" BOX	5.0	8.0	CB 2	TYPE "C" BOX	4.5	8.0	57	15	RCP	0.88%
CB 2	TYPE "C" BOX	4.0	8.0	CB 3	TYPE "C" BOX	3.0	5.7	50	15	RCP	2.00%
CB 3	TYPE "C" BOX	3.0	5.7	MES 1	MITERED END	2.2		49	15	RCP	1.63%
CB 3	TYPE "C" BOX	3.0	5.7	MES 2	MITERED END	2.2		60	15	RCP	1.33%
CB 4	TYPE "C" BOX	5.0	8.0	CB 5	TYPE "C" BOX	4.5	8.0	57	15	RCP	0.88%
CB 5	TYPE "C" BOX	4.0	8.0	CB 6	TYPE "C" BOX	3.0	5.7	50	15	RCP	2.00%
CB 6	TYPE "C" BOX	3.0	5.7	MES 3	MITERED END	2.2		33	15	RCP	2.42%
CB 6	TYPE "C" BOX	3.0	5.7	MES 4	MITERED END	2.2		47	15	RCP	1.70%
CB 7	CONTROL STRUCTURE	1.6	4.0	CB 7	TYPE "C" BOX	1.6		22	15	HDPE	0.00%
CB 7	TYPE "C" BOX	1.6	3.3	EX. OUTFALL	TYPE "C" BOX	1.6	3.27	22	15	HDPE	0.05%
CB 7	TYPE "C" BOX	1.6	3.3	MES	MITERED END	1.6		9	15	HDPE	0.00%

NOTE: DRY FLOOD PROOFING IS PROPOSED TO 11.17' NAVD



Reviewed for PW
Z. Luff
07/08/2024
3:34:32 PM

LEGEND

- PROPOSED ASPHALT
- DRY DETENTION
- PROPOSED 4" CONC.
- EXISTING GRADE
- PROPOSED GRADE
- MITERED END SECTION
- CATCH BASIN

DESIGN	BAF	5			
DRAWN	AOM	4			
CHECKED	BAF	3			
		2			
D.C.	NO.	DATE	REVISION	APPROVED BY	

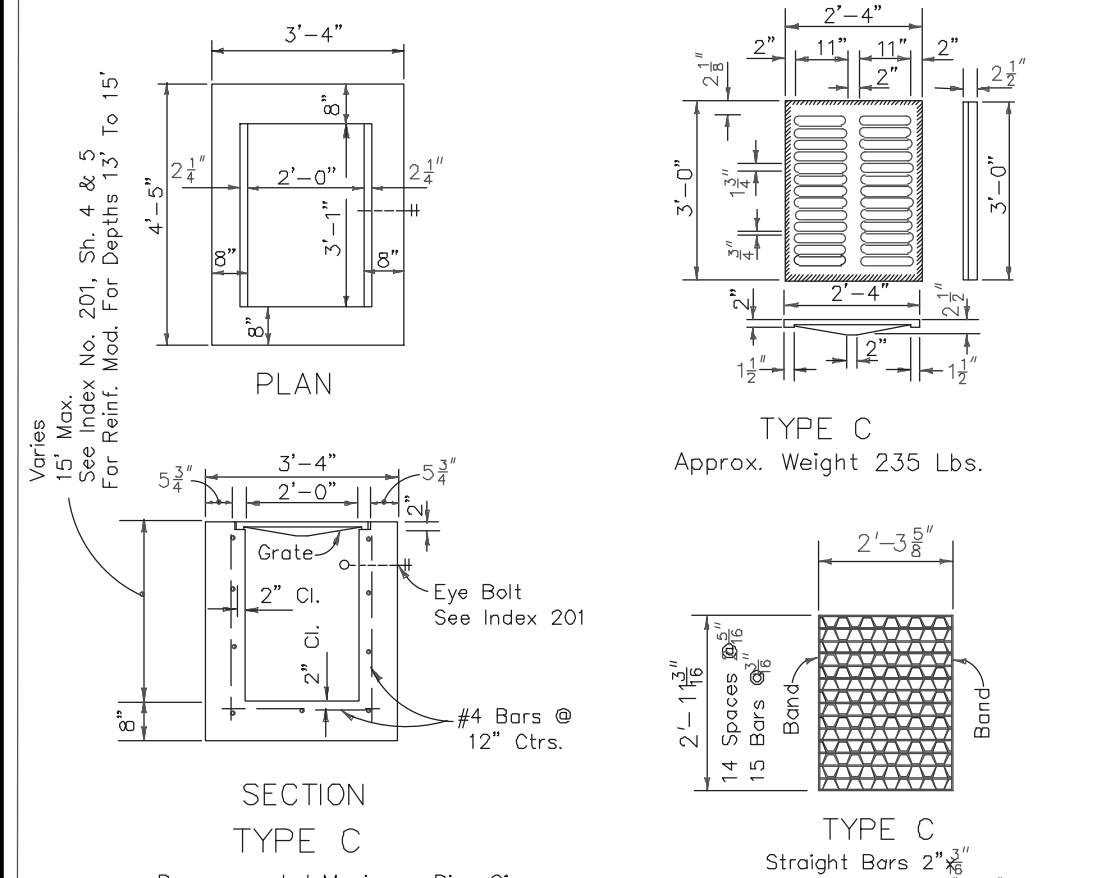
NOT VALID FOR CONSTRUCTION UNLESS SIGNED IN THIS BLOCK

Blair A. Foley, P.E., LLC
Civil Engineer / Development Consultant
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Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols00@aol.com

CLIENT: MARCO LUXE LLC
MARCO LUXE
Collier County, Florida

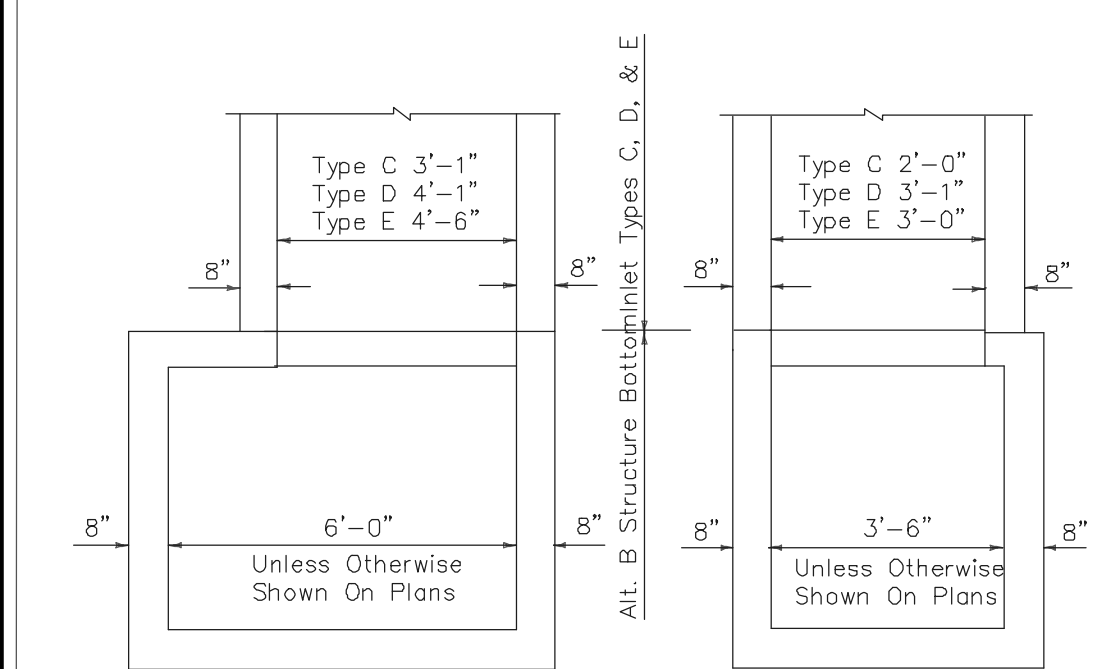
PAVING, GRADING AND DRAINAGE PLAN

PROJ. NO. SCALE 1" = 30'
DATE 4/9/2024
SHT 5 of 8



SECTION TYPE C
Recommended Maximum Pipe Size:
2'-0" Wall-18" Pipe
3'-1" Wall-24" Pipe

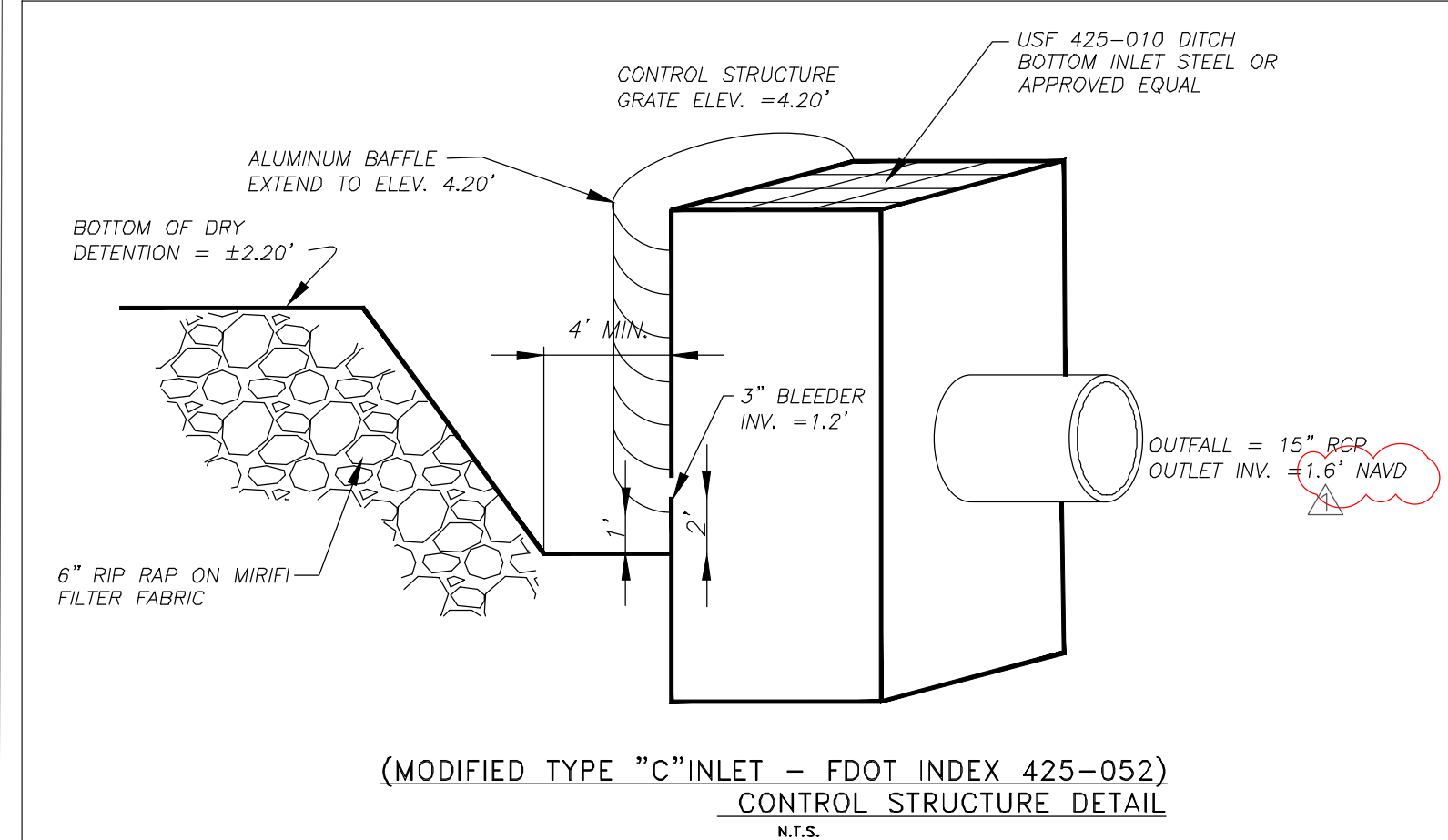
NOTE: Steel Grates Are Required On Inlets With Traversable Slots And On Inlets where Bicycle Traffic Is Anticipated.



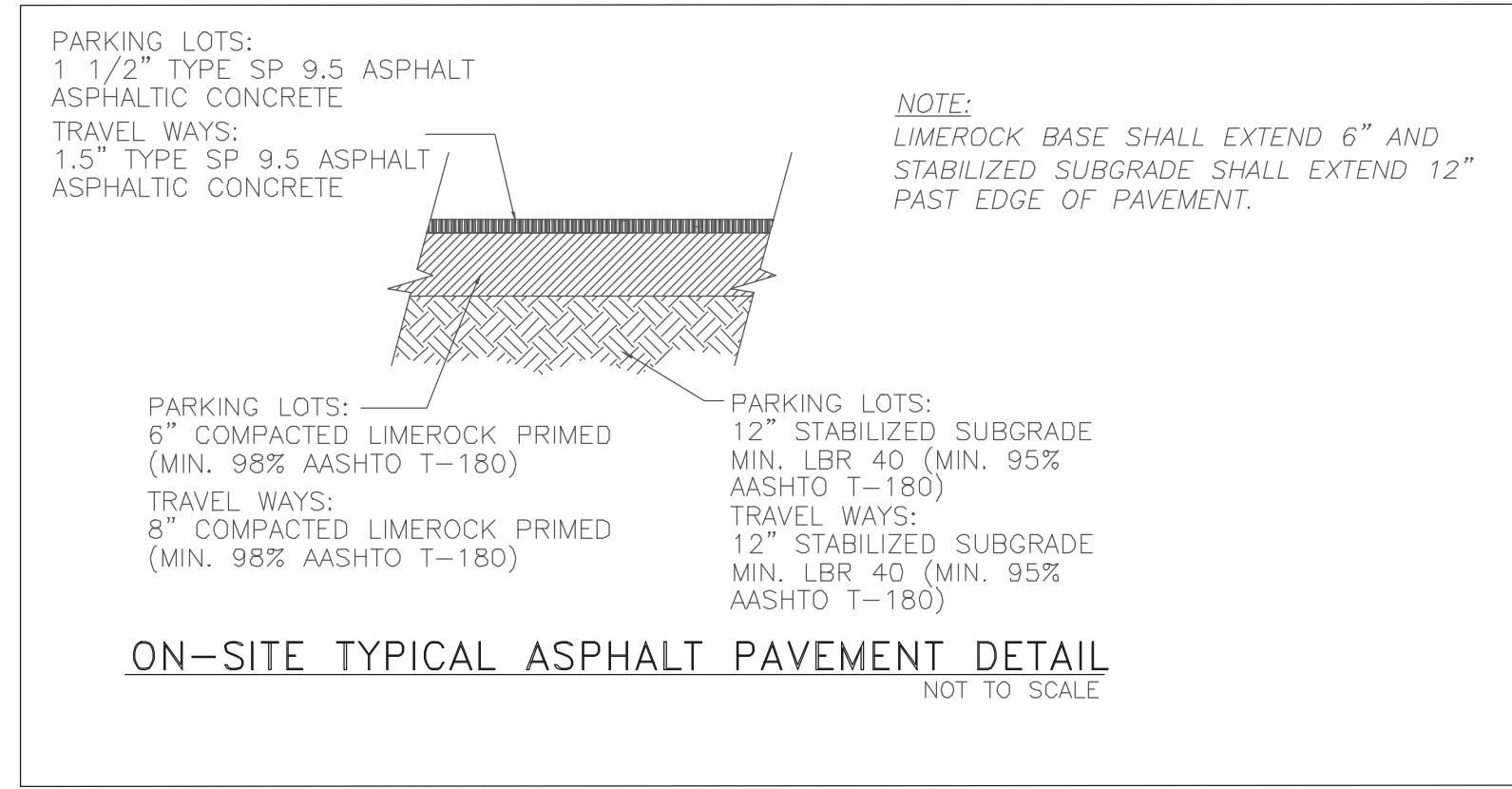
NOTE: Alt. B Structure Bottom Only. See Index No. 200
STRUCTURE BOTTOM FOR INLETS TYPE C, D & E

GENERAL NOTES

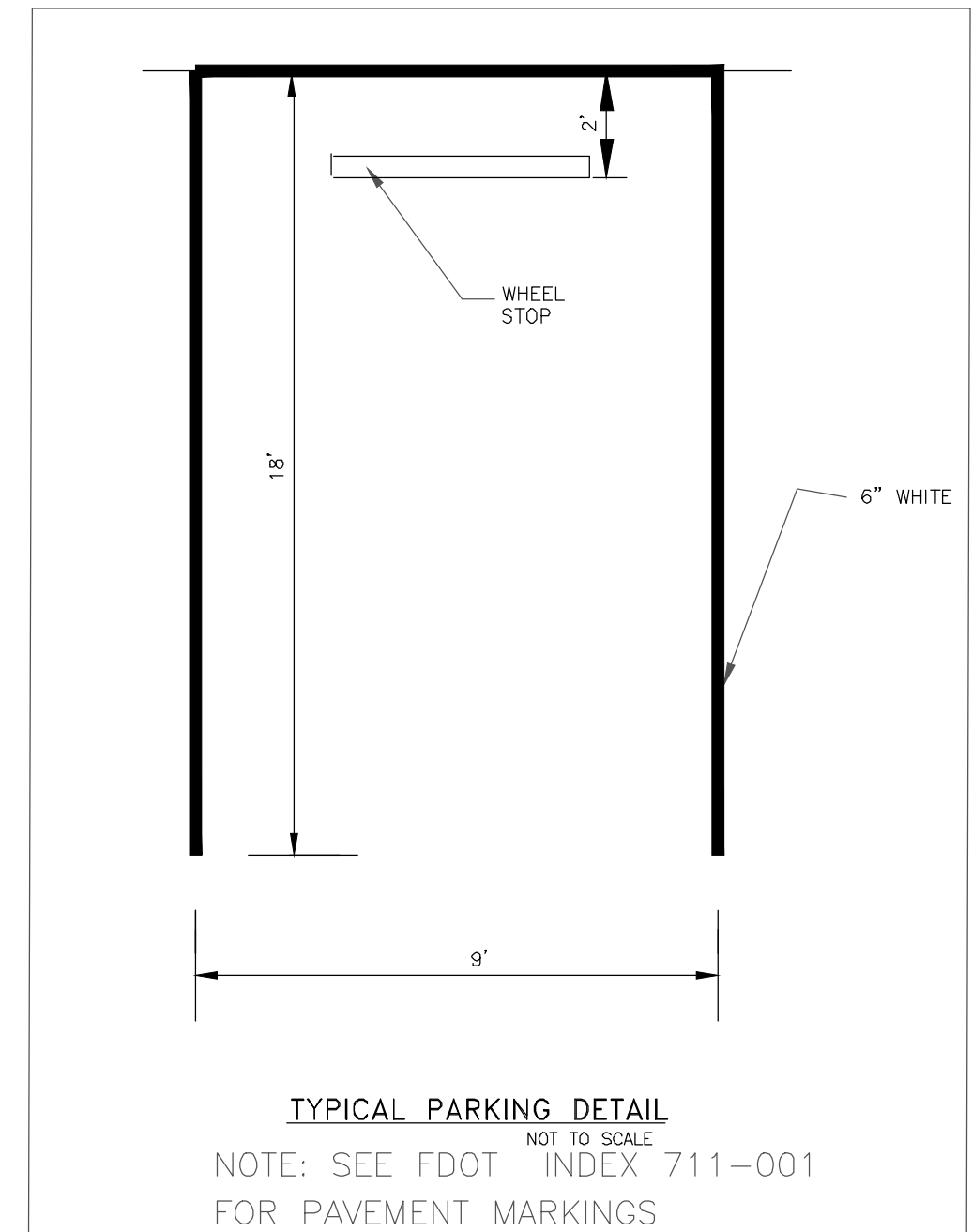
- These inlets are suitable for bicycle and pedestrian areas and are to be used in ditches, medians and other areas subject to infrequent traffic loadings but are not to be placed in areas subject to any heavy wheel loads.
- Inlets subject to minimal debris should be constructed without slots. Where debris is a problem inlets should be constructed with slots. Slotted inlets located within roadway clear zones and in areas accessible to pedestrians shall have traversable slots. The traversable slot modification is not adoptable to inlet Type H. Slots may be constructed at either or both ends as shown on plans.
- Steel grates are to be used on all inlets where bicycle traffic is anticipated. Steel grates are to be used on all inlets with traversable slots. Either cast iron or steel grates may be used on inlets without slots where bicycle traffic is not anticipated. Either cast iron or steel grates may be used on all inlets with non-traversable slots. Subject to the selection described above, when Alternate G grate is specified in the plans, either the steel grate, hot dipped galvanized after fabrication, or the cast iron grate may be used, unless the plans stipulate the particular type.
- Recommended maximum pipe sizes shown are for concrete pipe. Pipe sizes larger than those recommended must be checked for fit.
- All exposed corners and edges of concrete are to be chamfered 3/4".
- Pavement to be used on inlets without slots and inlets with non-traversable slots only when called for in the plans; but required on all traversable slot inlets. Cost to be included in contract unit price for inlets. Quantities shown are for information only.
- Traversable slots constructed in existing inlets shall be paid for as inlets partial, and shall include the cost for slot openings, paving and any required replacement grates.
- Sodding to be used on all inlets not located in paved areas and paid for under contract unit price for Sodding SY.
- For supplementary details see Index No. 425-001.



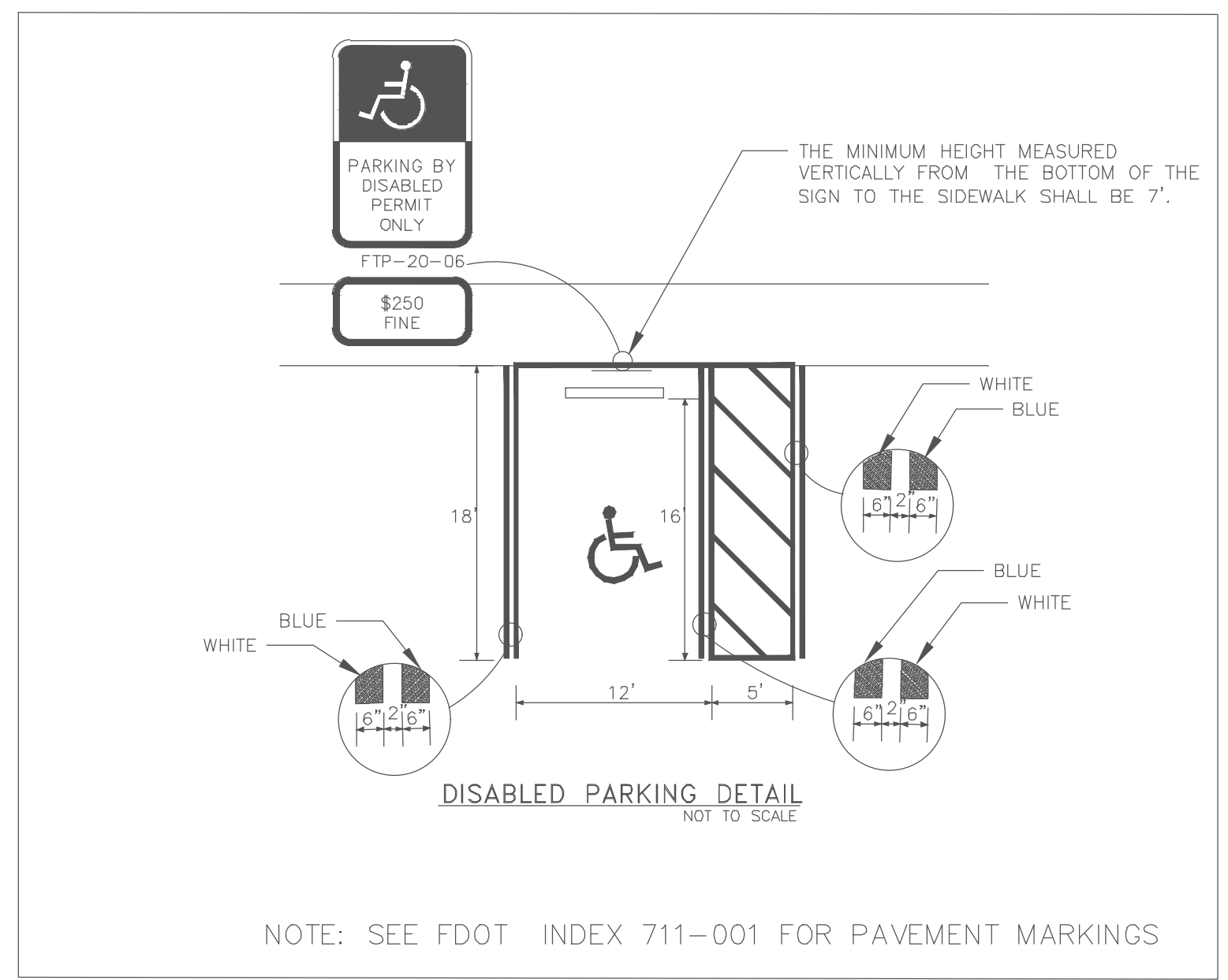
(MODIFIED TYPE "C" INLET - FDOT INDEX 425-052)
CONTROL STRUCTURE DETAIL
N.T.S.



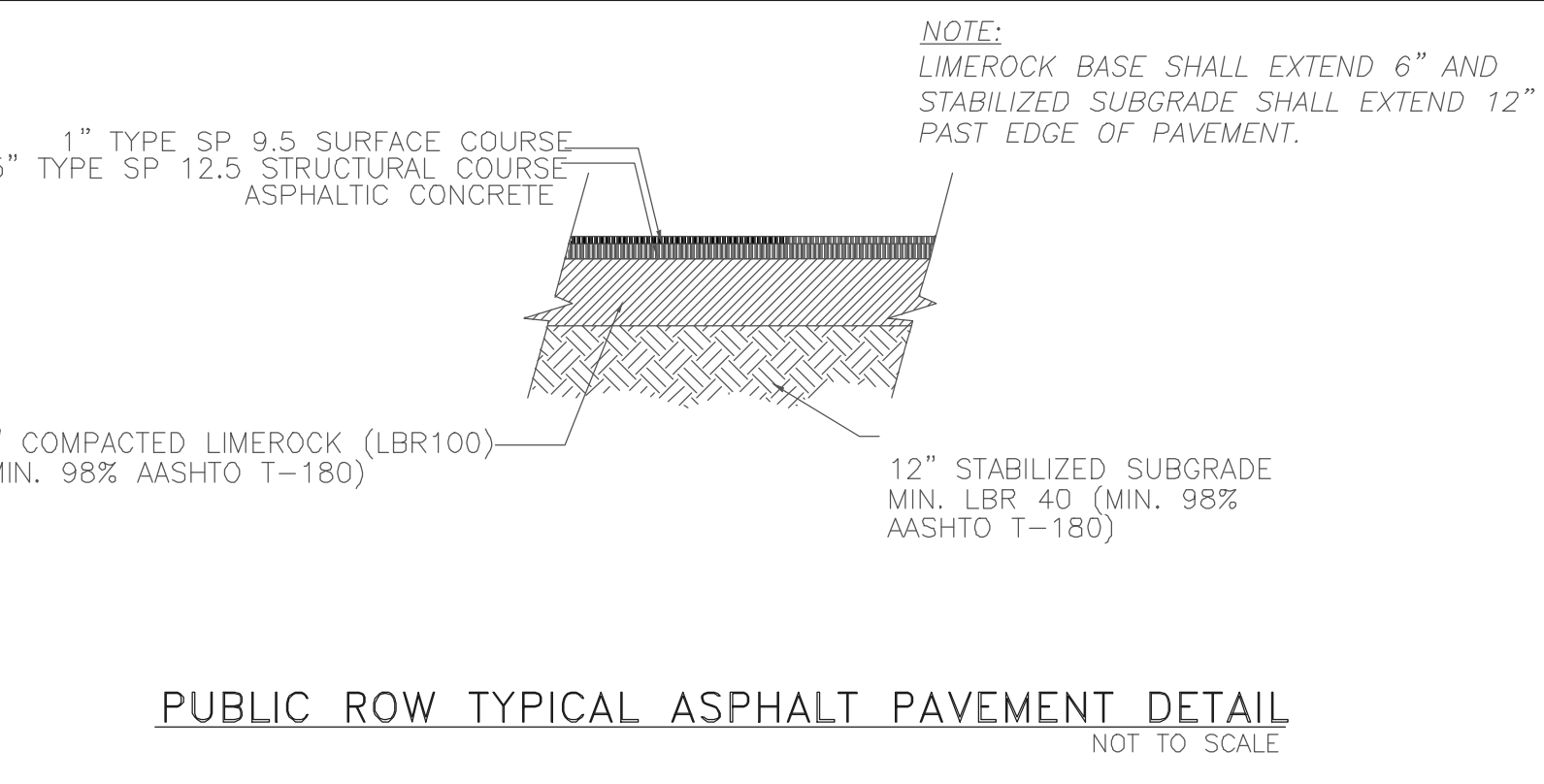
ON-SITE TYPICAL ASPHALT PAVEMENT DETAIL
NOT TO SCALE



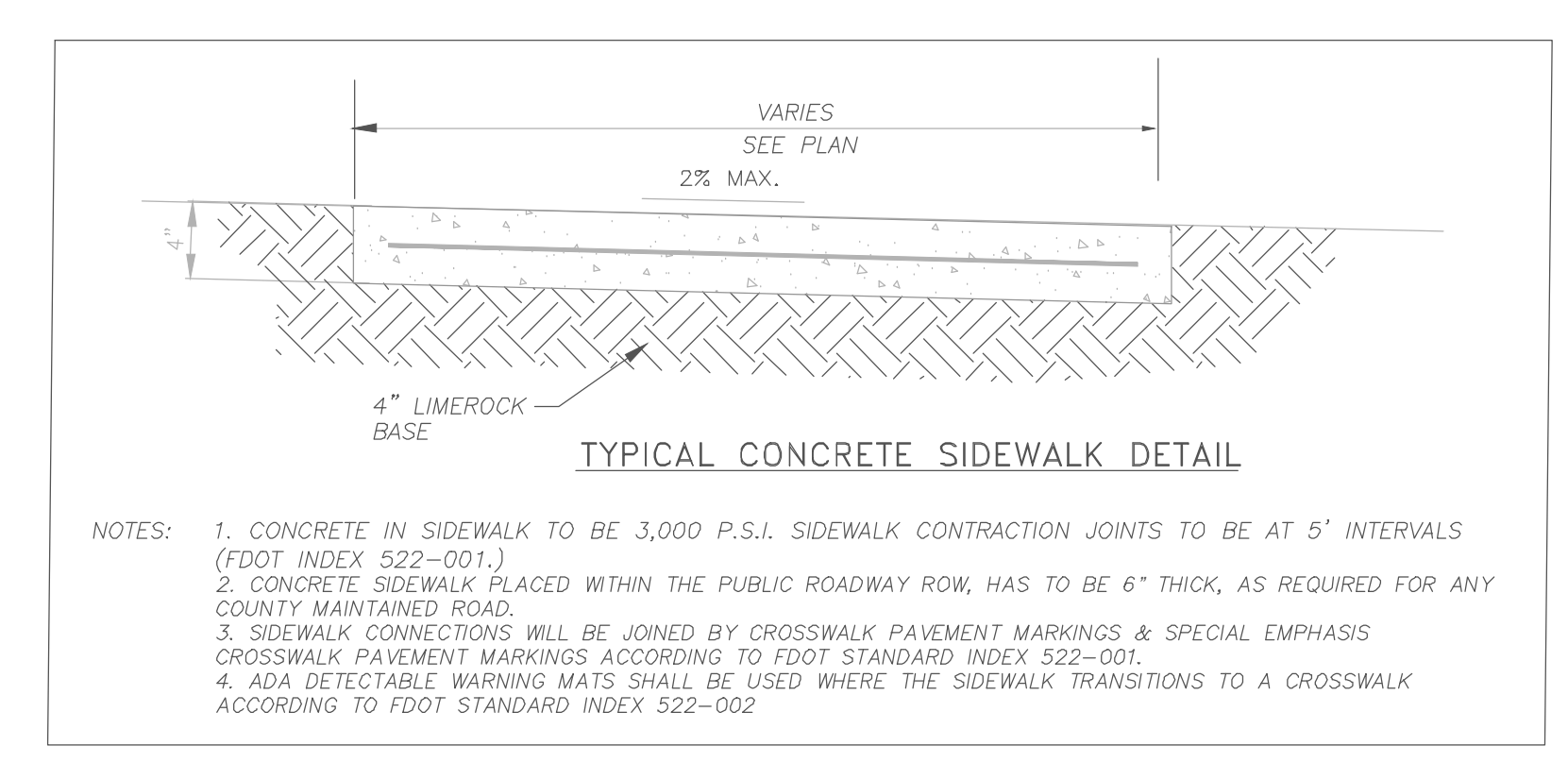
TYPICAL PARKING DETAIL
NOTE: SEE FDOT INDEX 711-001 FOR PAVEMENT MARKINGS
NOT TO SCALE



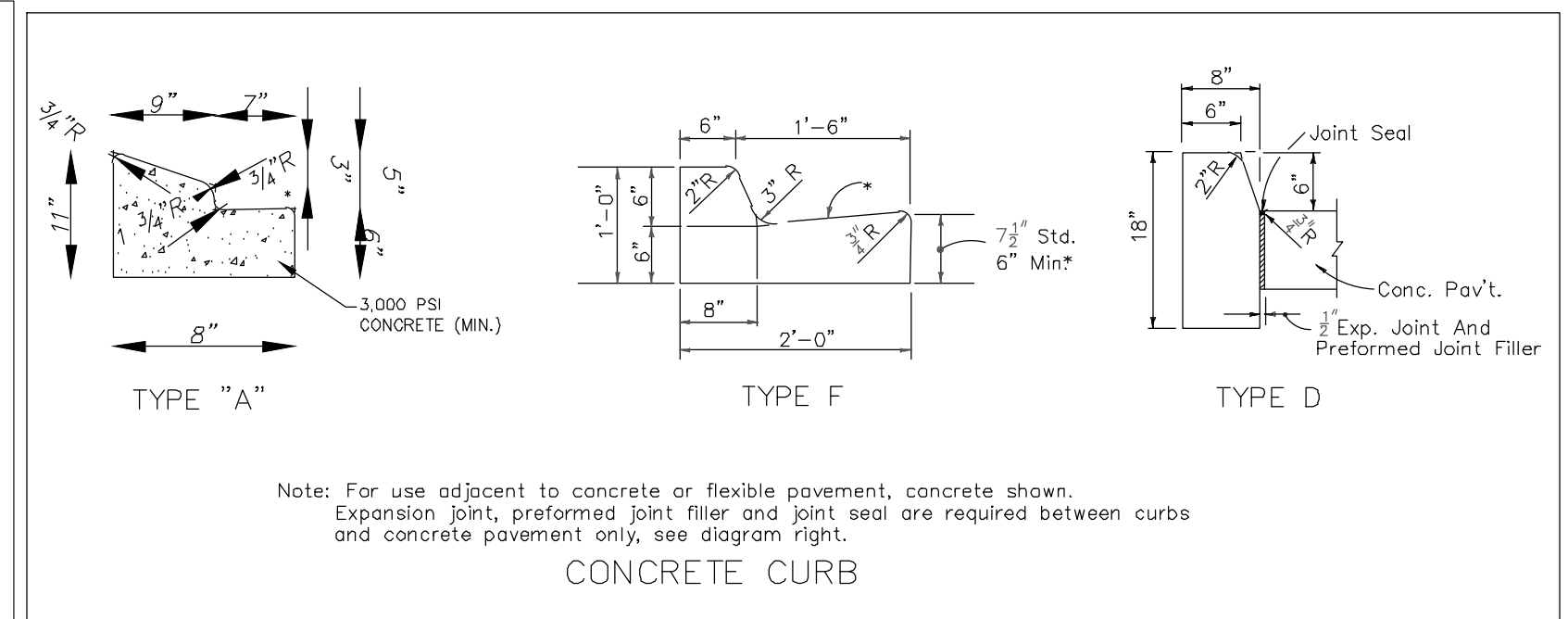
DISABLED PARKING DETAIL
NOTE: SEE FDOT INDEX 711-001 FOR PAVEMENT MARKINGS
NOT TO SCALE



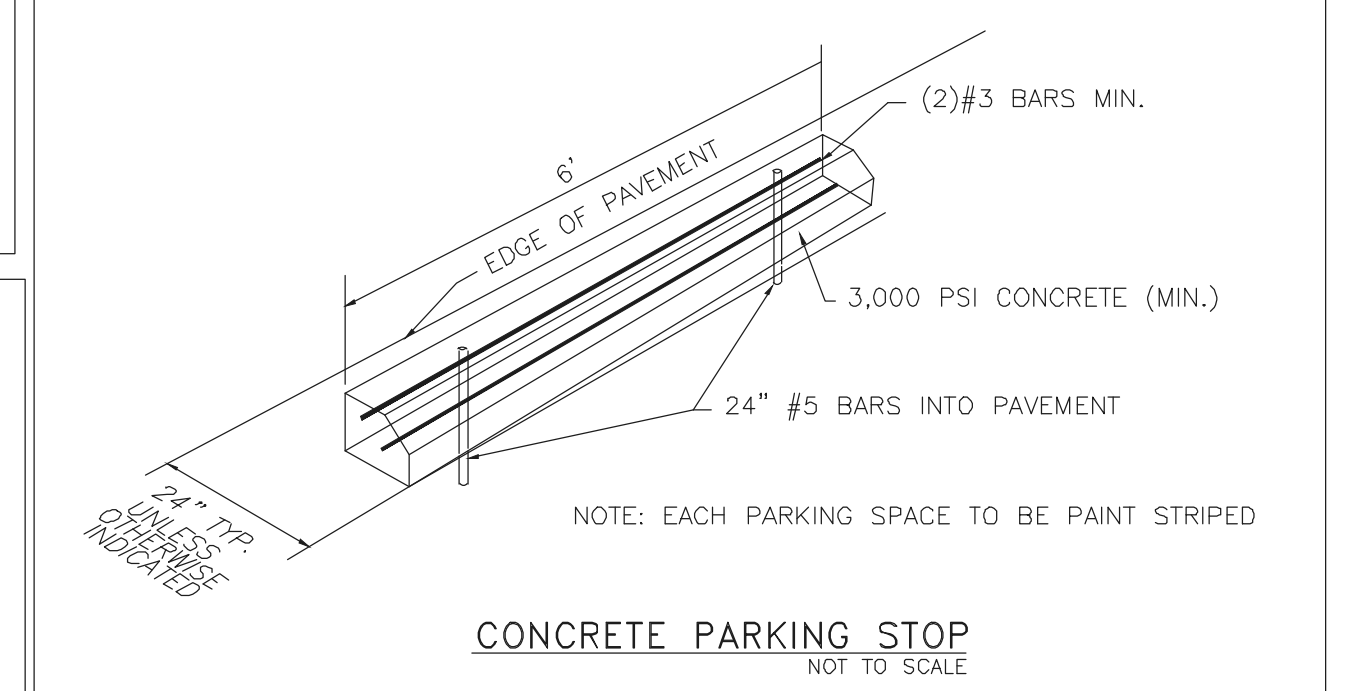
PUBLIC ROW TYPICAL ASPHALT PAVEMENT DETAIL
NOT TO SCALE



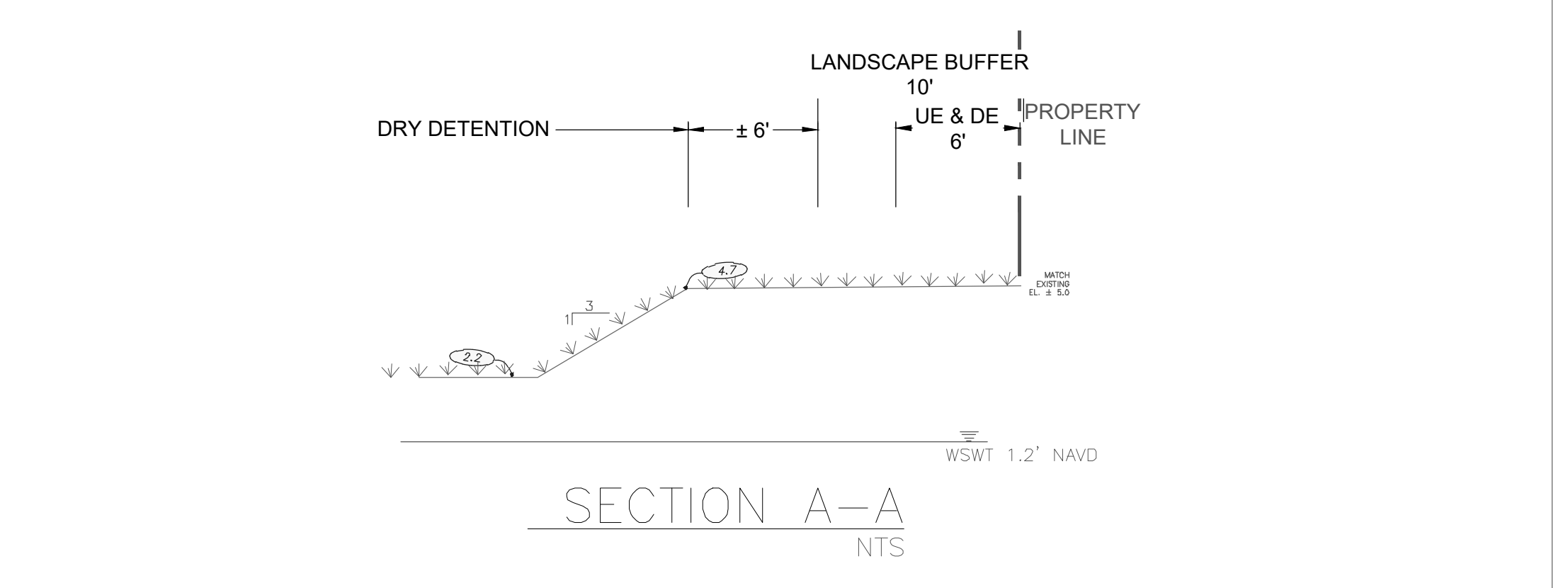
TYPICAL CONCRETE SIDEWALK DETAIL
NOTES:
1. CONCRETE IN SIDEWALK TO BE 3,000 P.S.I. SIDEWALK CONTRACTION JOINTS TO BE AT 5' INTERVALS (FDOT INDEX 522-001.)
2. CONCRETE SIDEWALK PLACED WITHIN THE PUBLIC ROADWAY ROW, HAS TO BE 6" THICK, AS REQUIRED FOR ANY COUNTY MAINTAINED ROAD.
3. SIDEWALK CONNECTIONS WILL BE JOINED BY CROSSWALK PAVEMENT MARKINGS & SPECIAL EMPHASIS CROSSWALK PAVEMENT MARKINGS ACCORDING TO FDOT STANDARD INDEX 522-001.
4. ADA DETECTABLE WARNING MATS SHALL BE USED WHERE THE SIDEWALK TRANSITIONS TO A CROSSWALK ACCORDING TO FDOT STANDARD INDEX 522-002
NOT TO SCALE



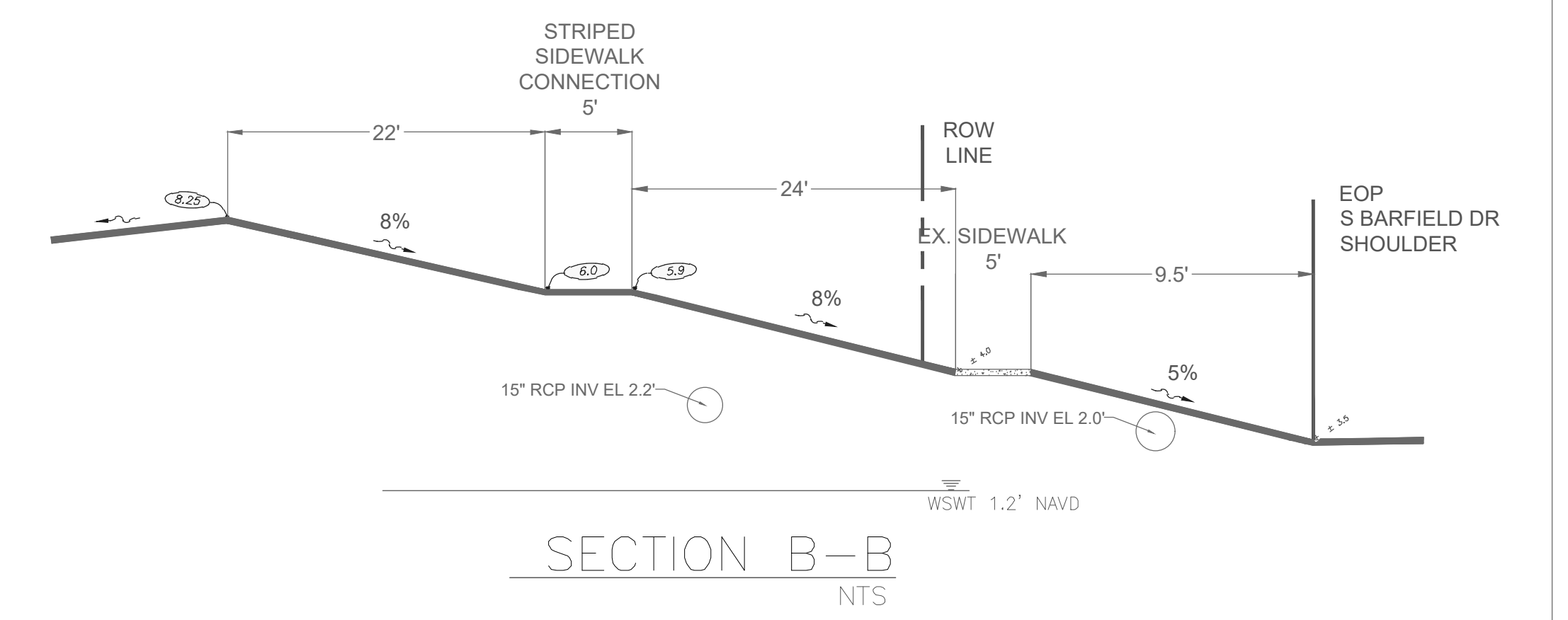
CONCRETE CURB
NOTE: For use adjacent to concrete or flexible pavement, concrete shown. Expansion joint, preformed joint filler and joint seal are required between curbs and concrete pavement only, see diagram right.



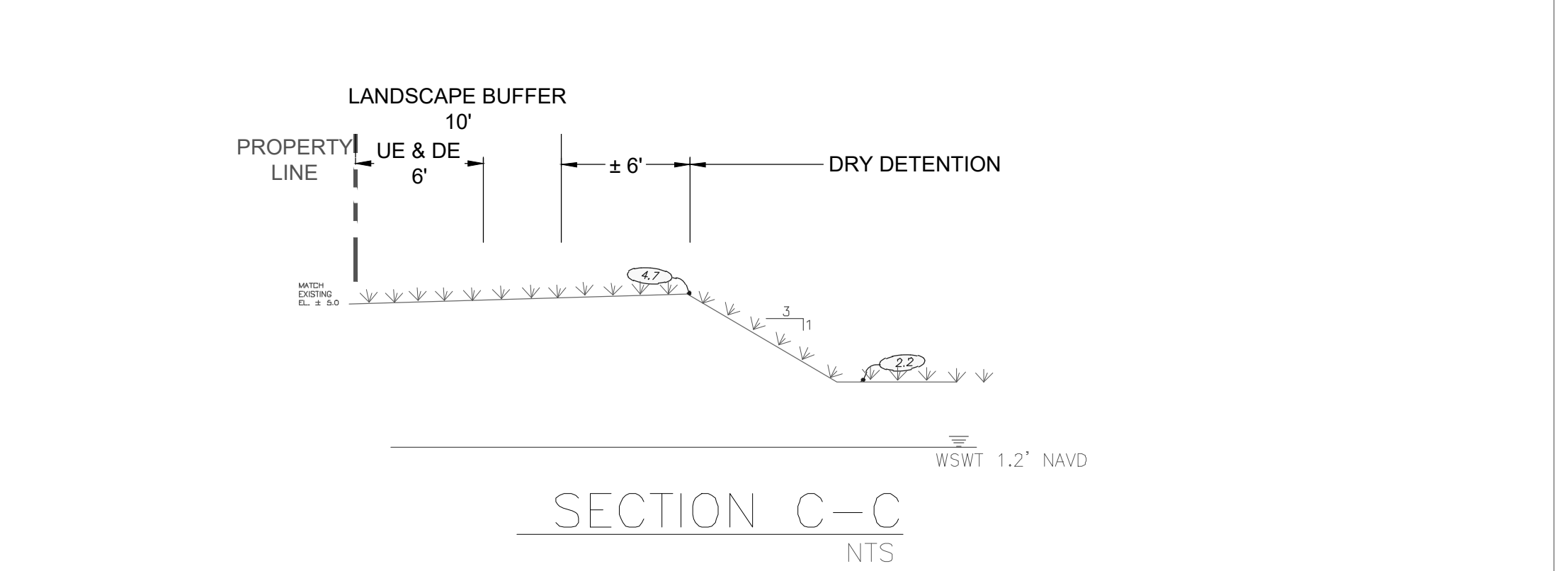
CONCRETE PARKING STOP
NOT TO SCALE



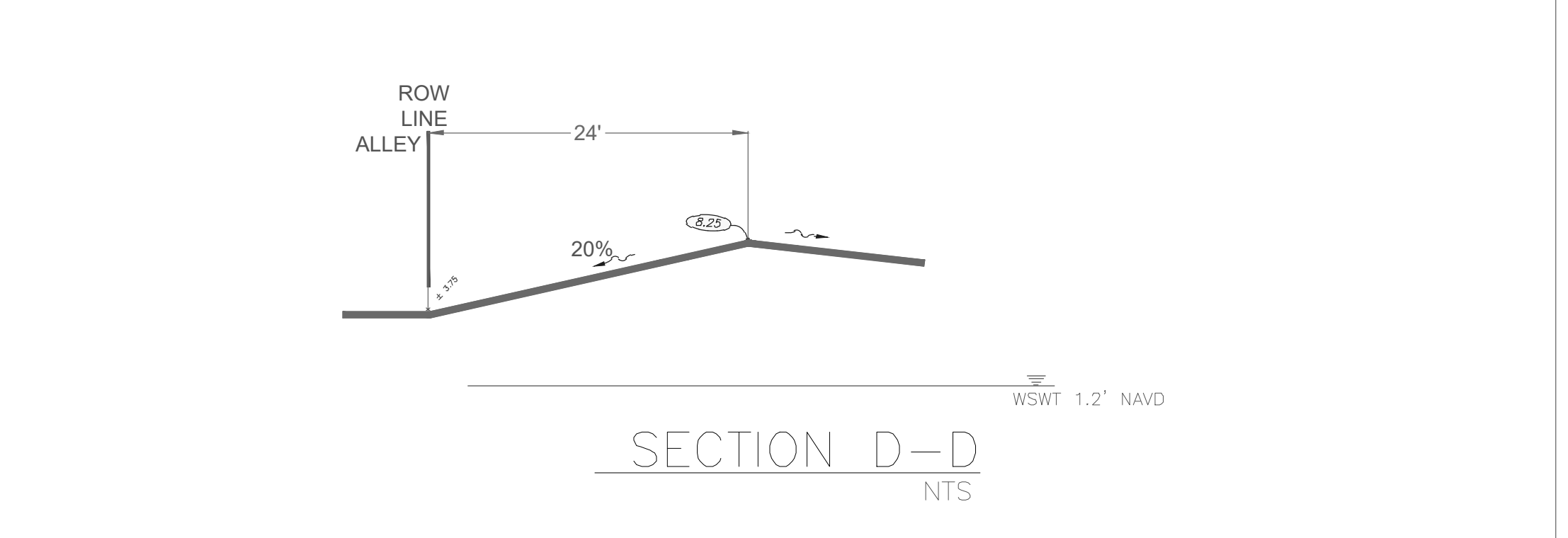
SECTION A-A
NTS



SECTION B-B
NTS



SECTION C-C
NTS



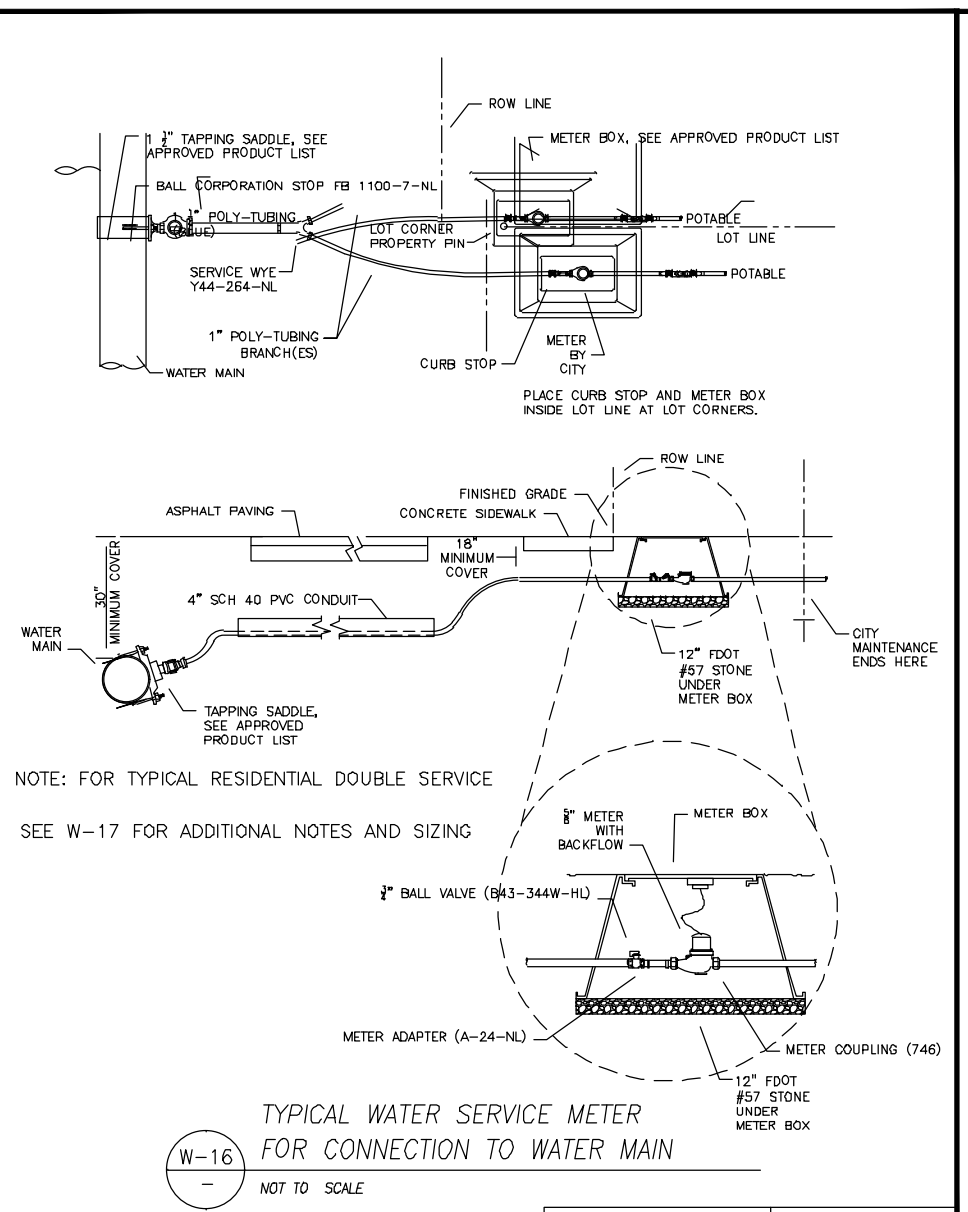
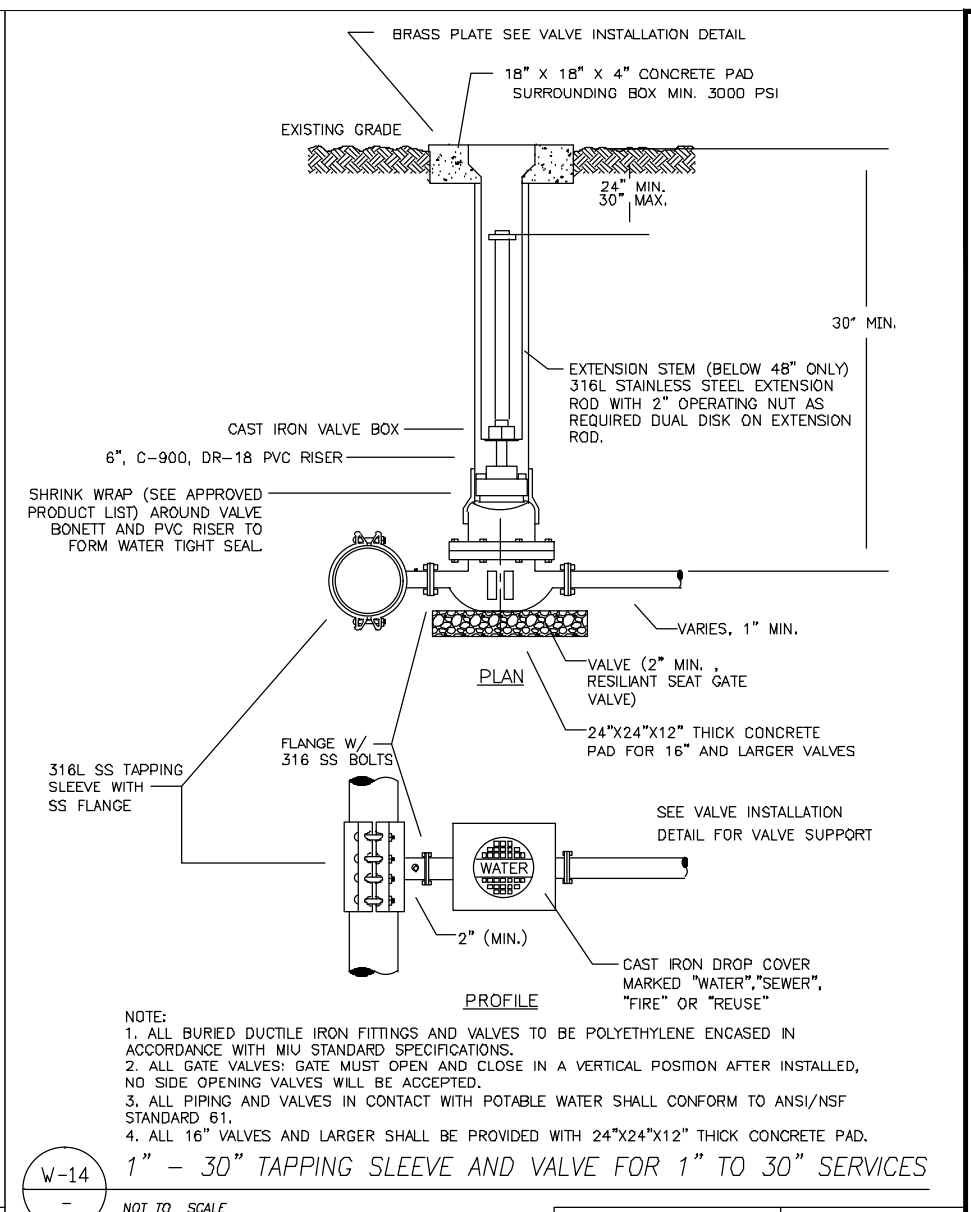
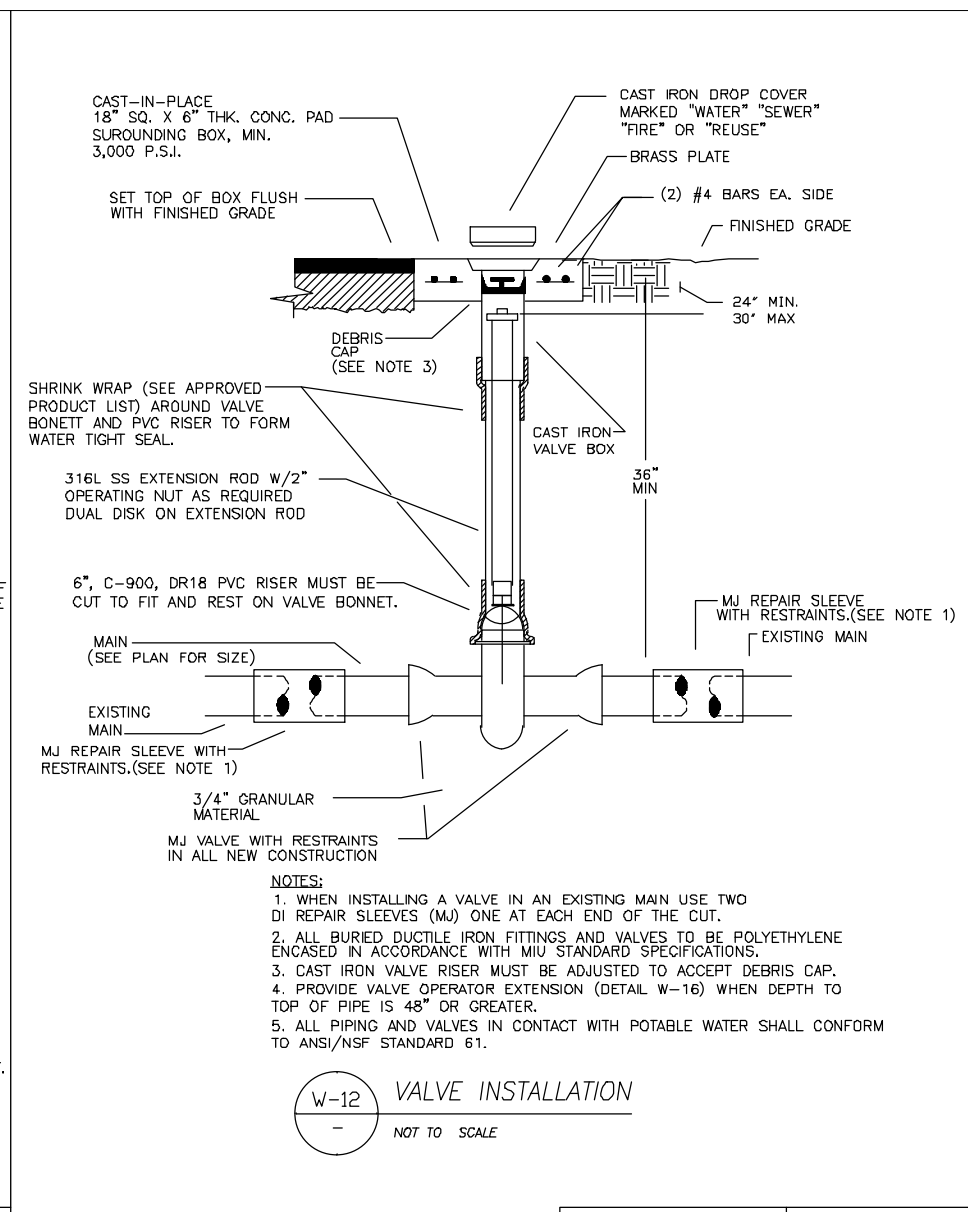
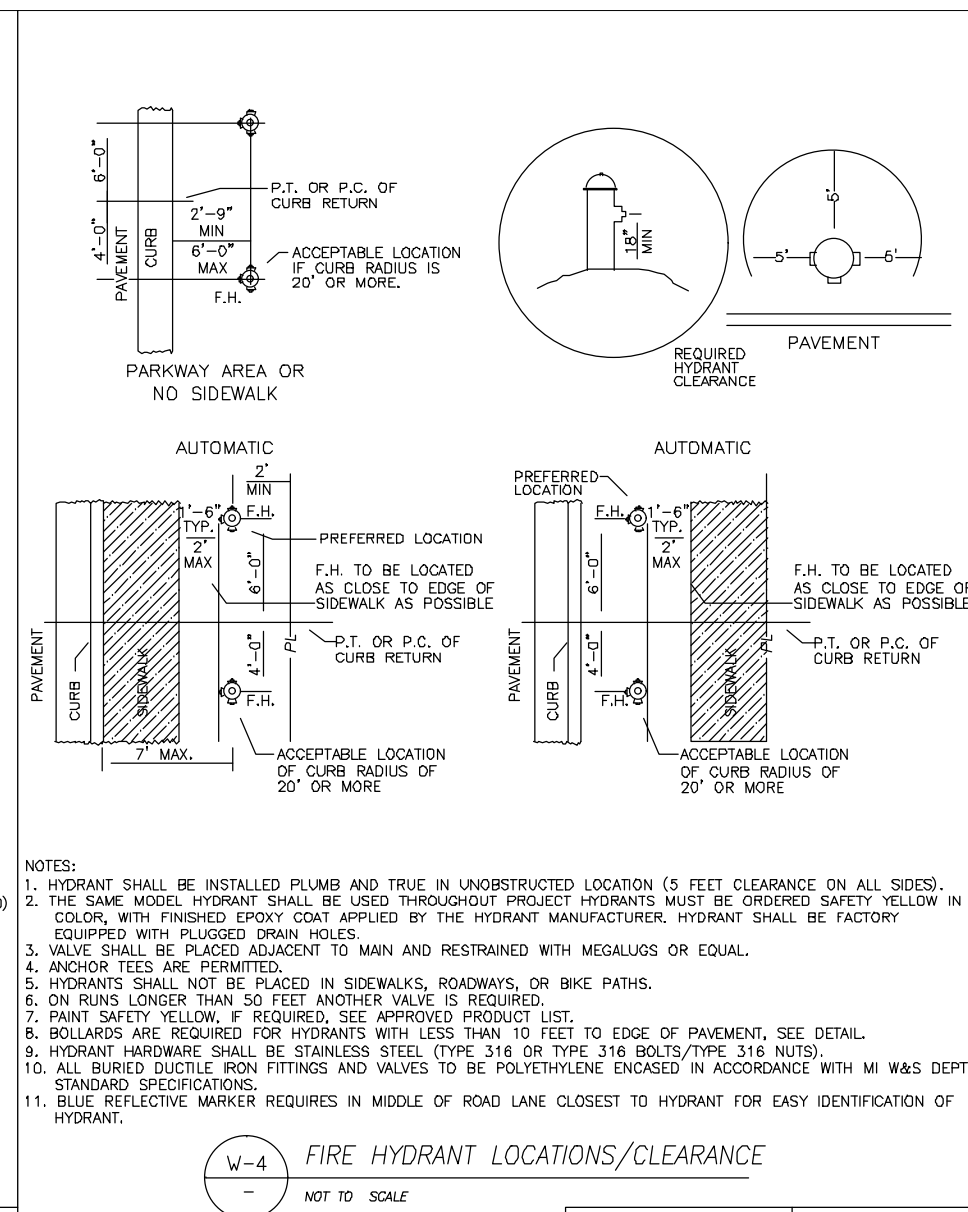
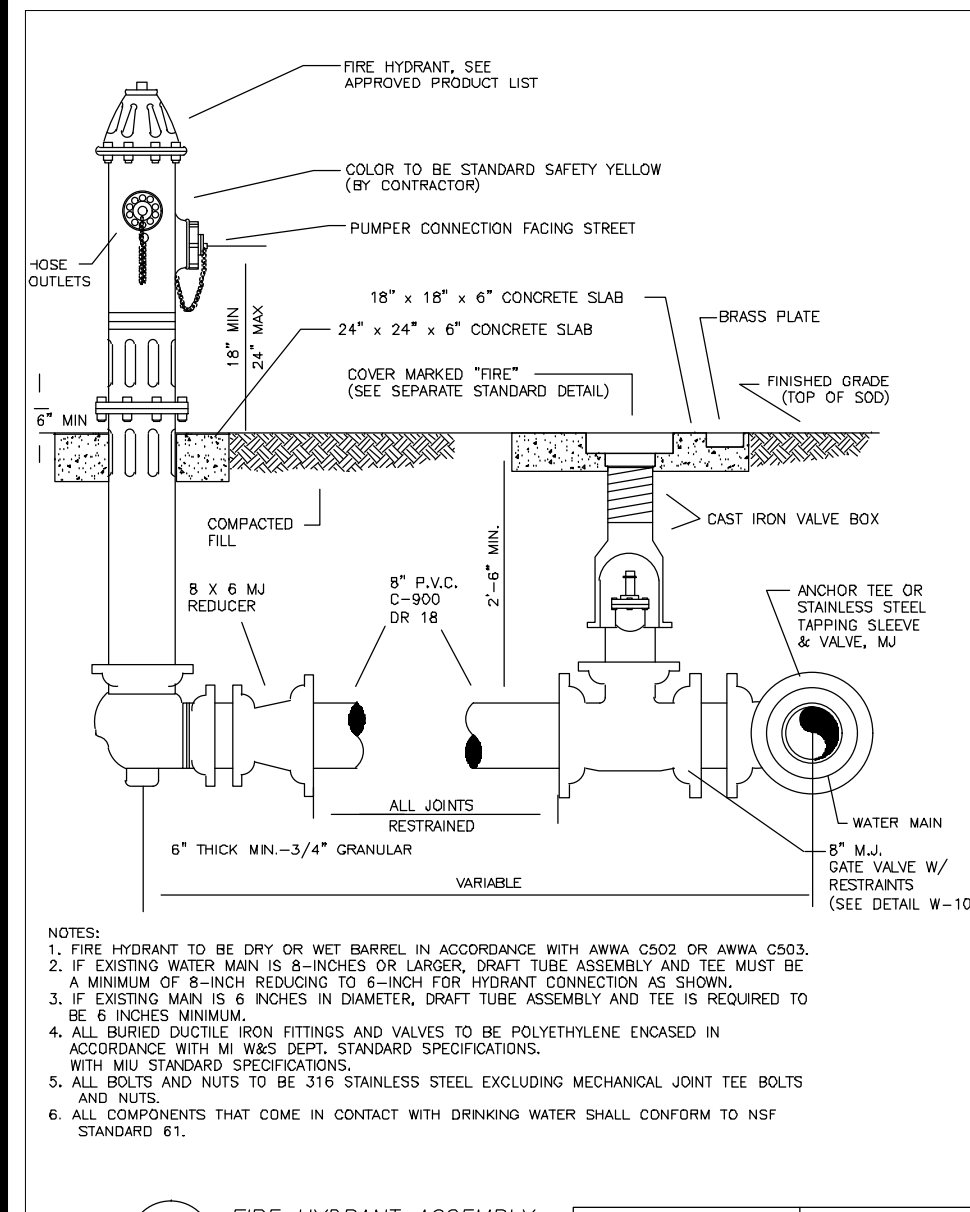
SECTION D-D
NTS

DESIGN	BAF	5			
DRAWN	AOM	4			
CHECKED	BAF	3			
		2			
		1	6/27/24	City of Marco Comment Letter dated 6/11/24	BAF
C.C.		NO.	DATE	REVISION	APPD BY

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Civil Engineer / Development Consultant
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CLIENT: MARCO LUXE LLC
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Collier County, Florida

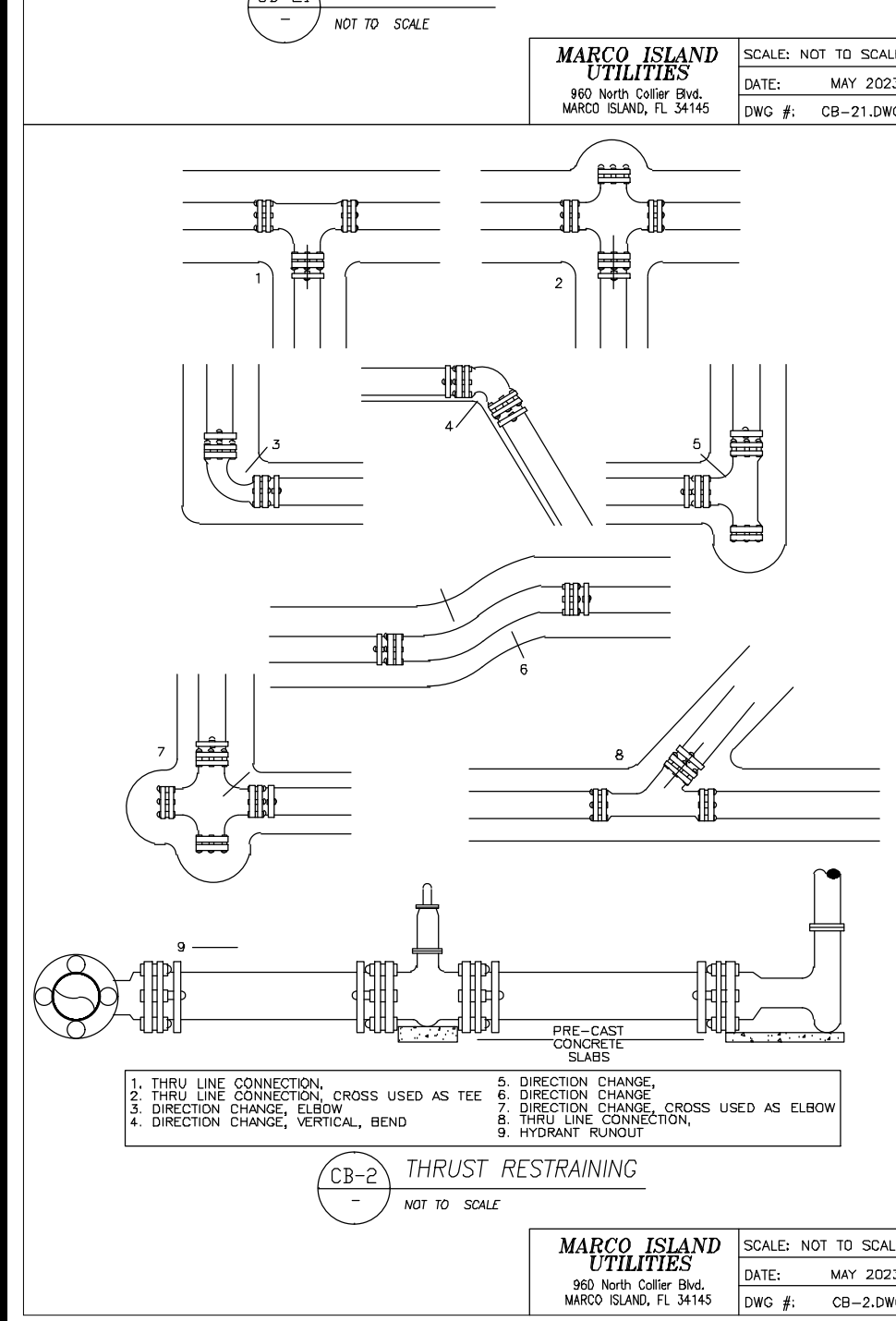
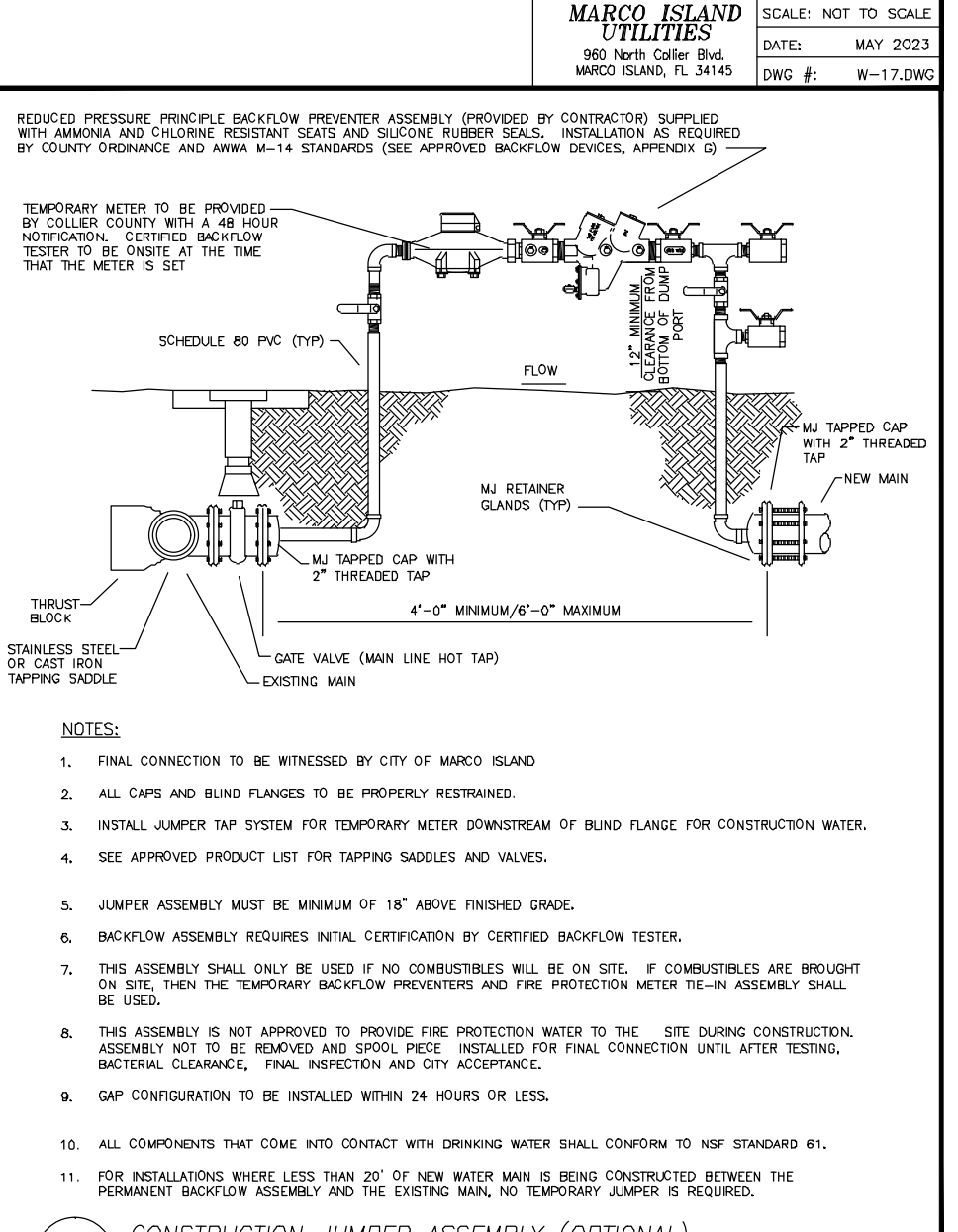
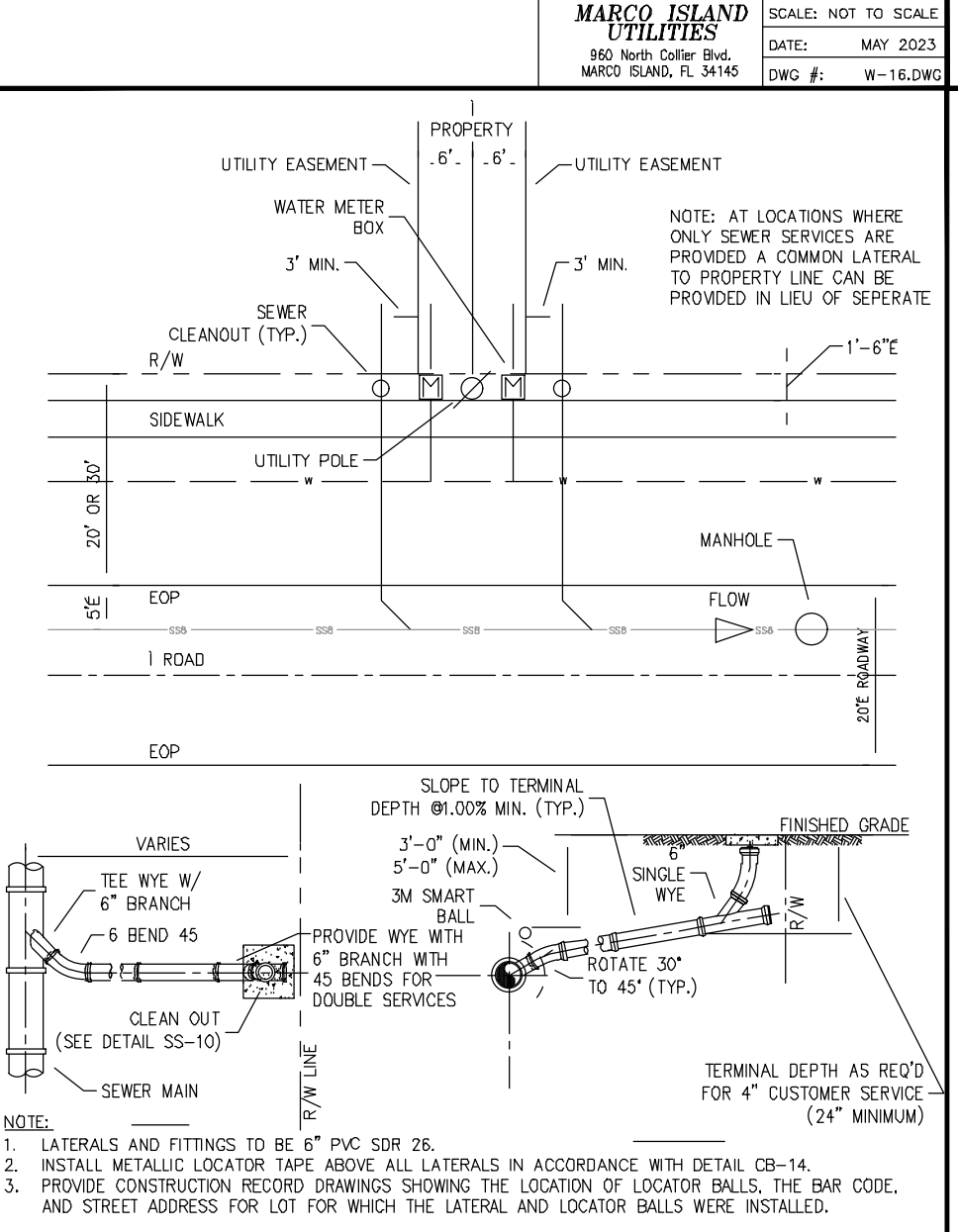
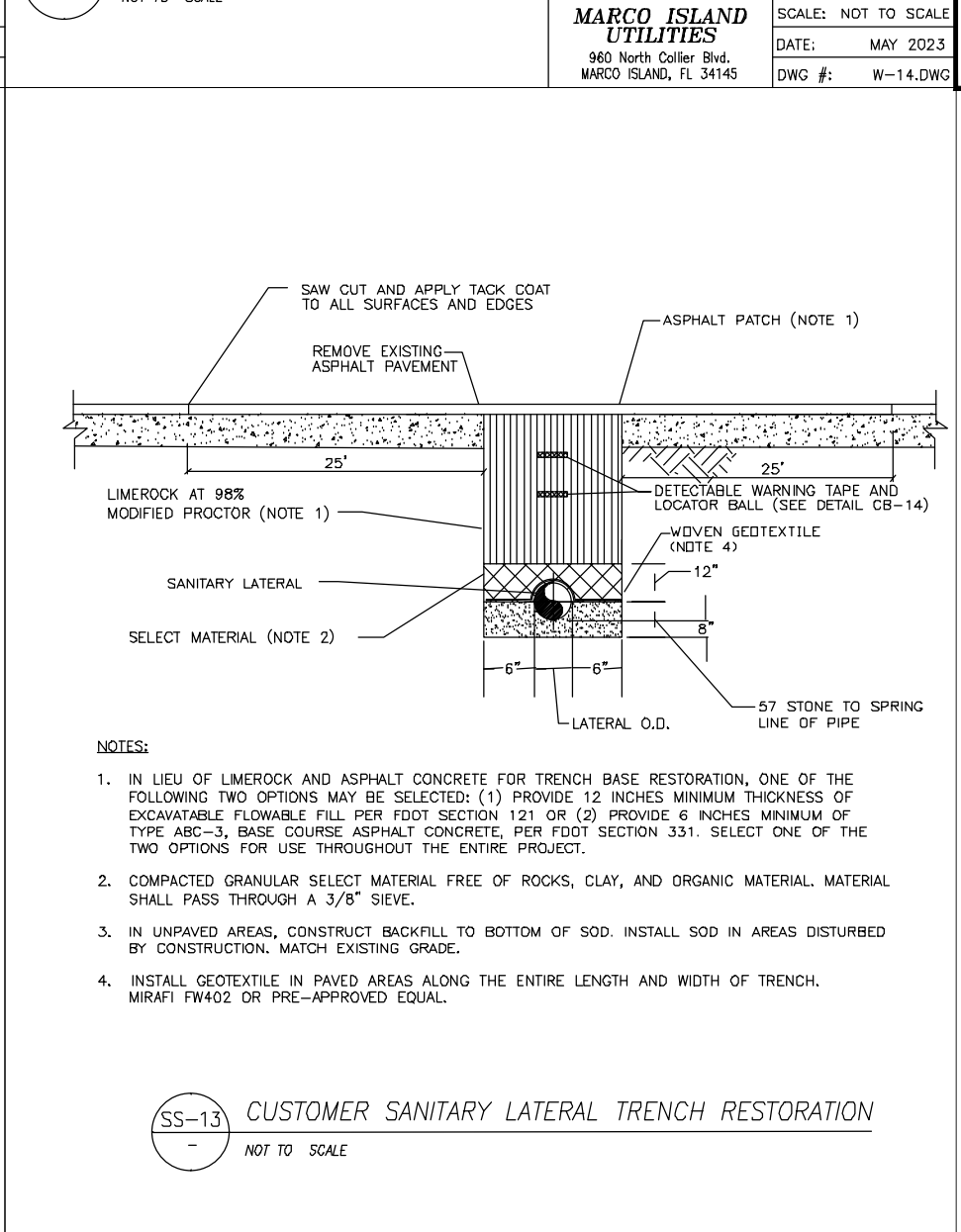
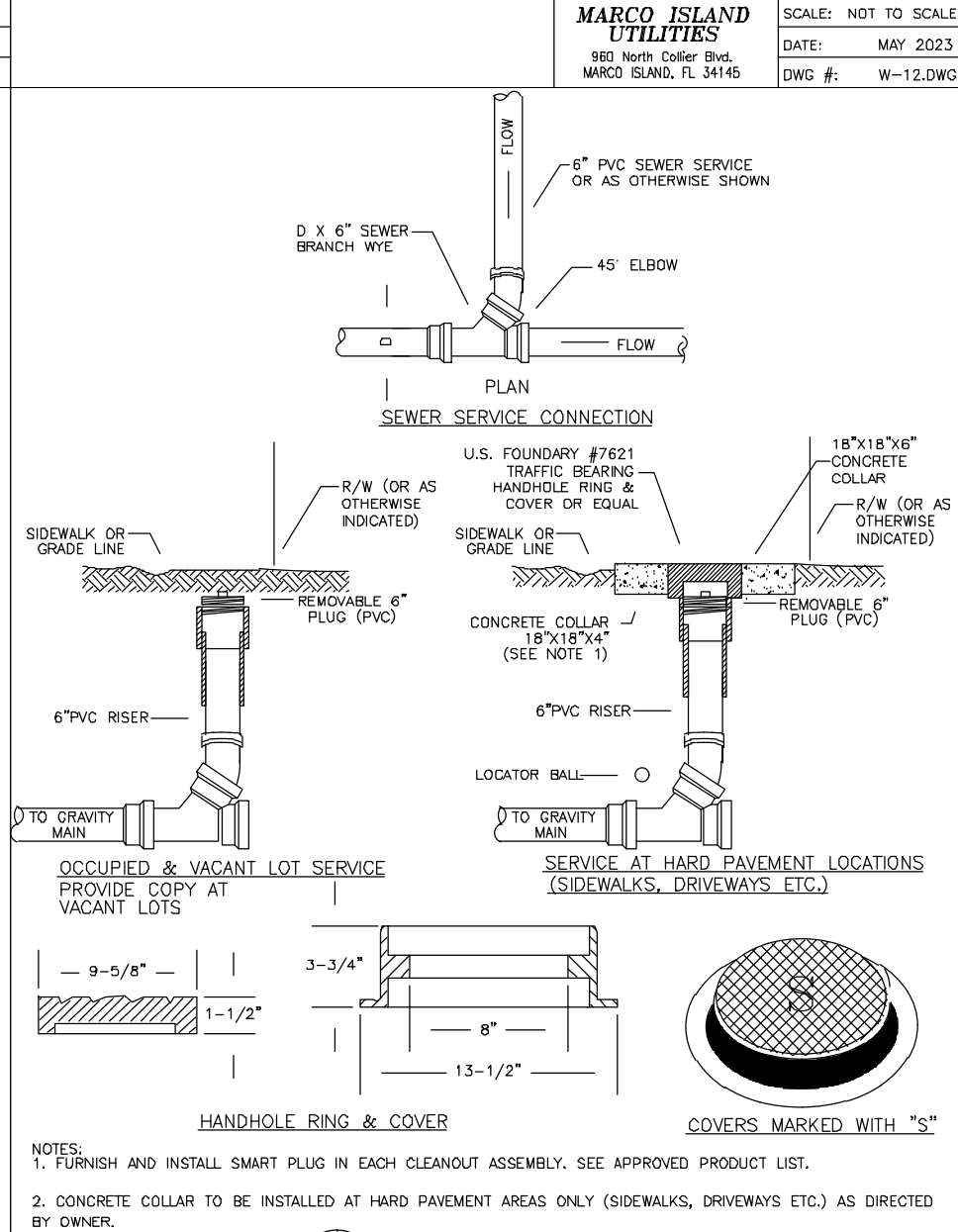
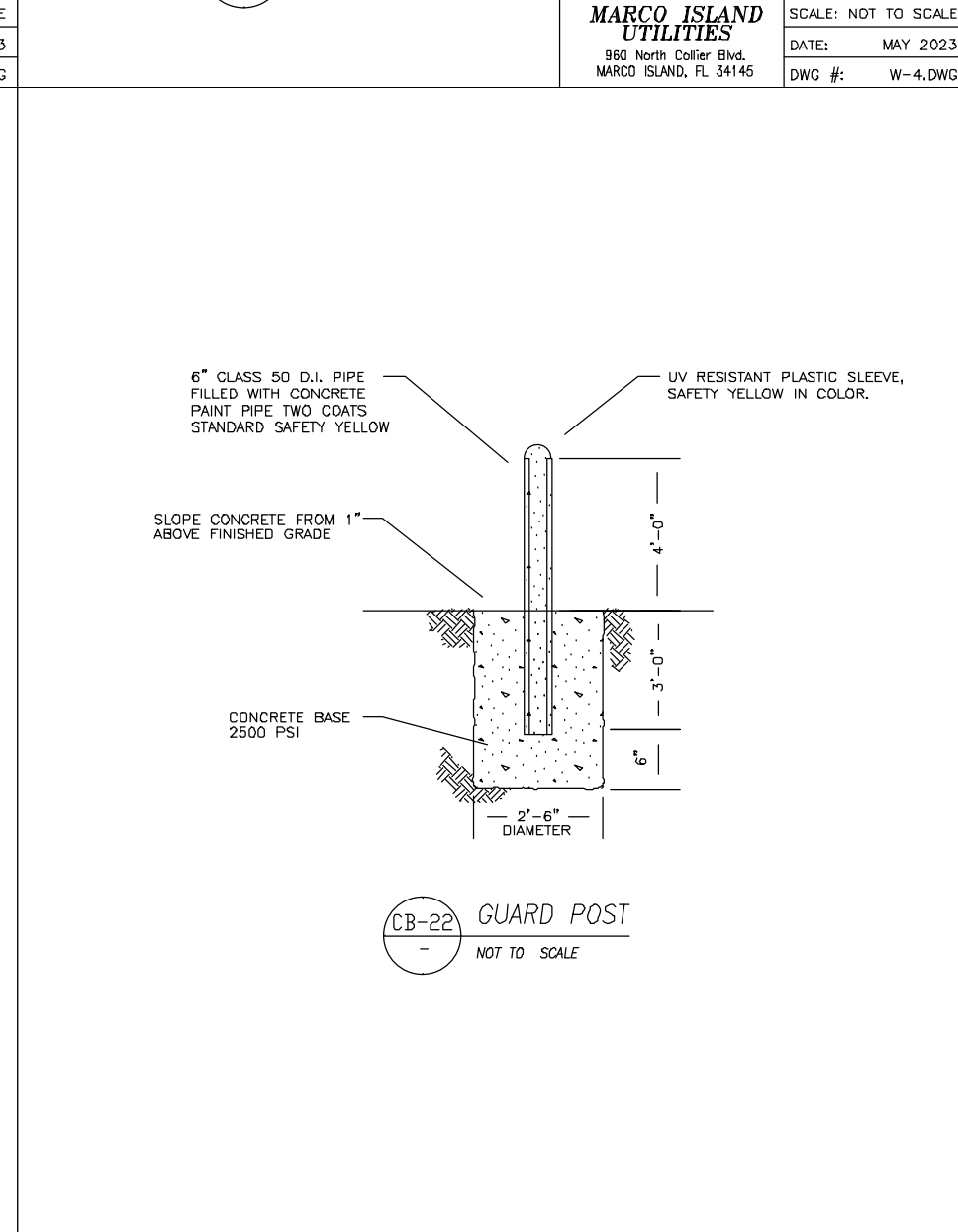
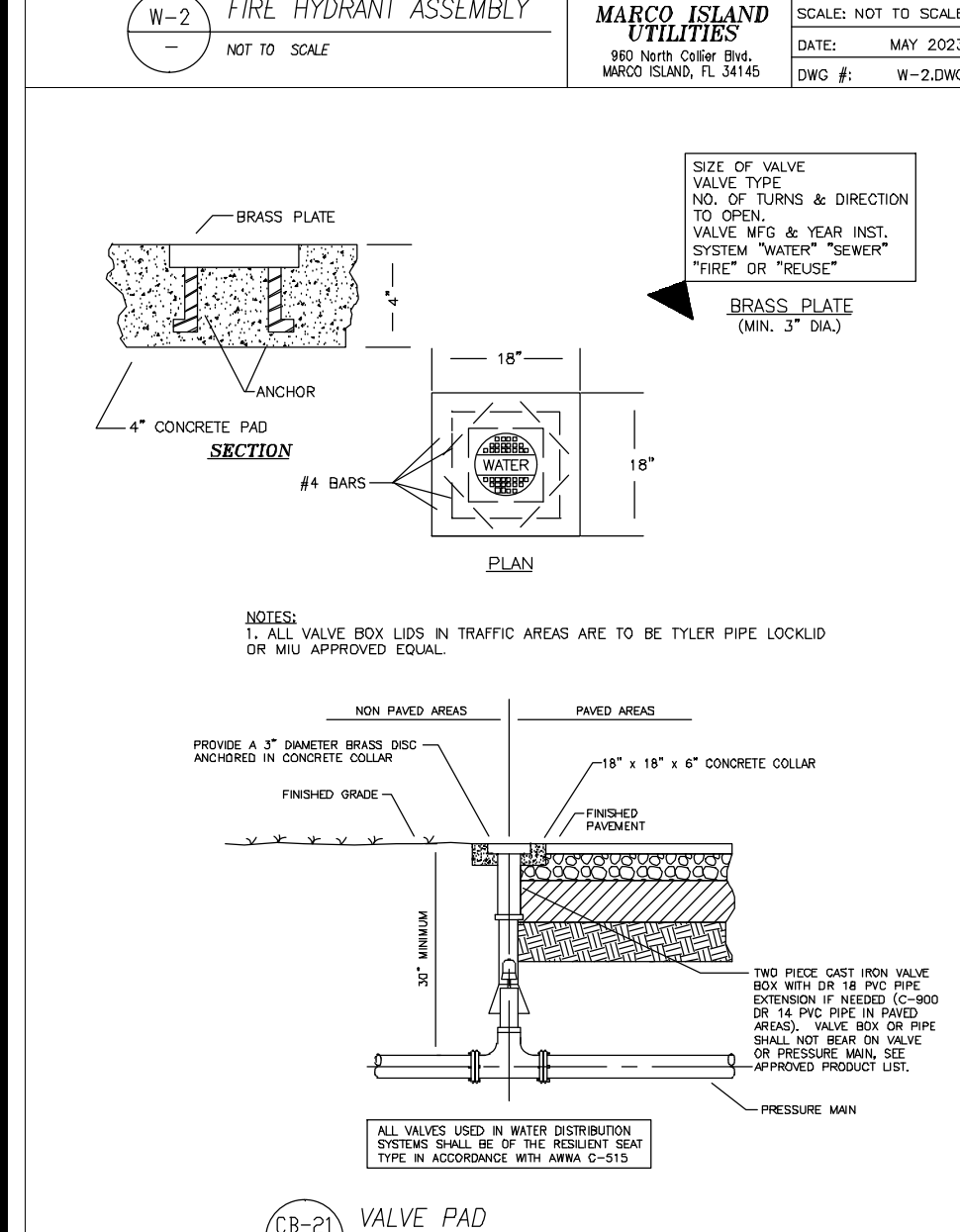
DETAILS & SECTIONS
PROJ. NO. N.T.S.
SCALE N.T.S.
DATE 4/9/2024
SHT 6 OF 8



SERVICE CONNECTION SIZING CHART

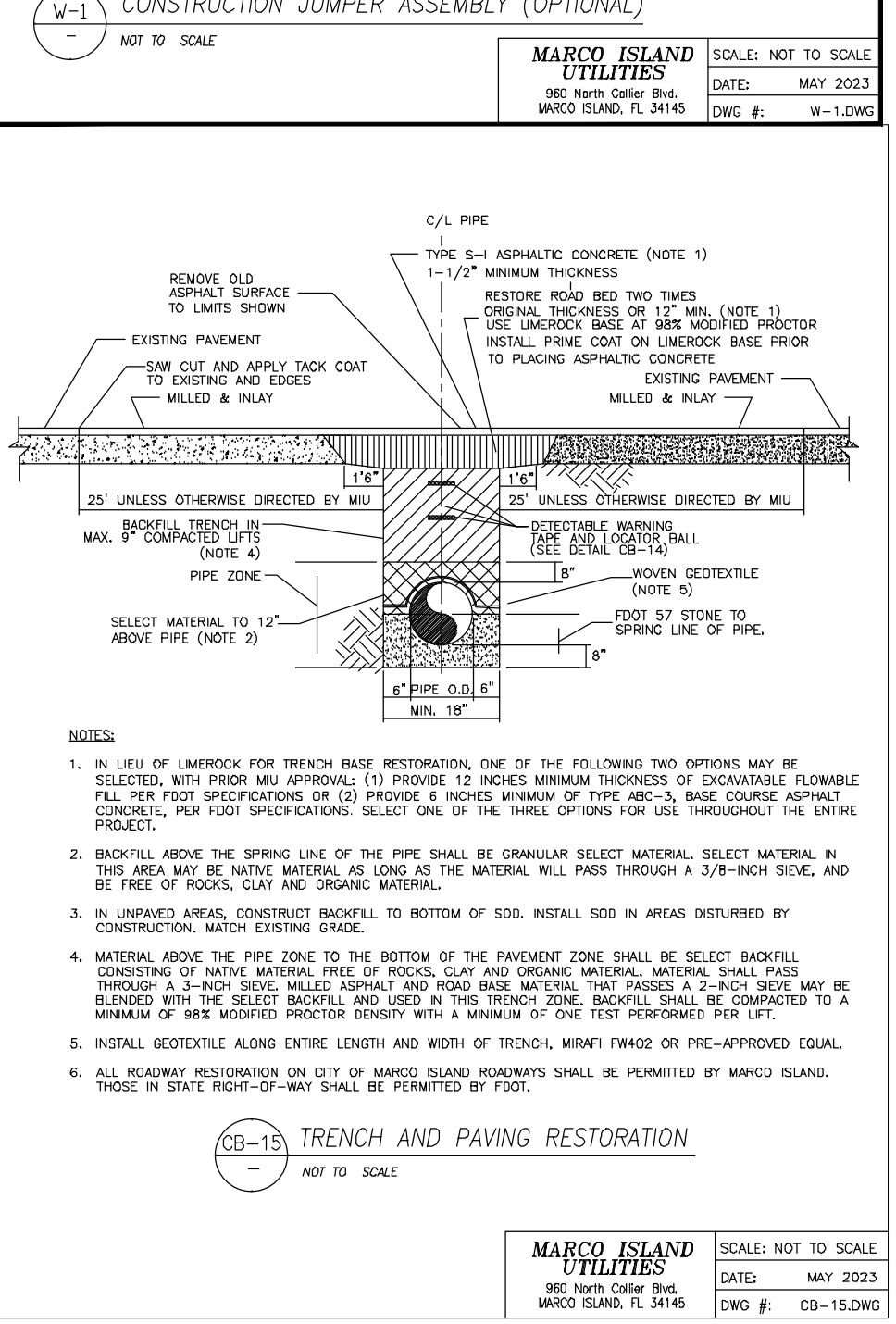
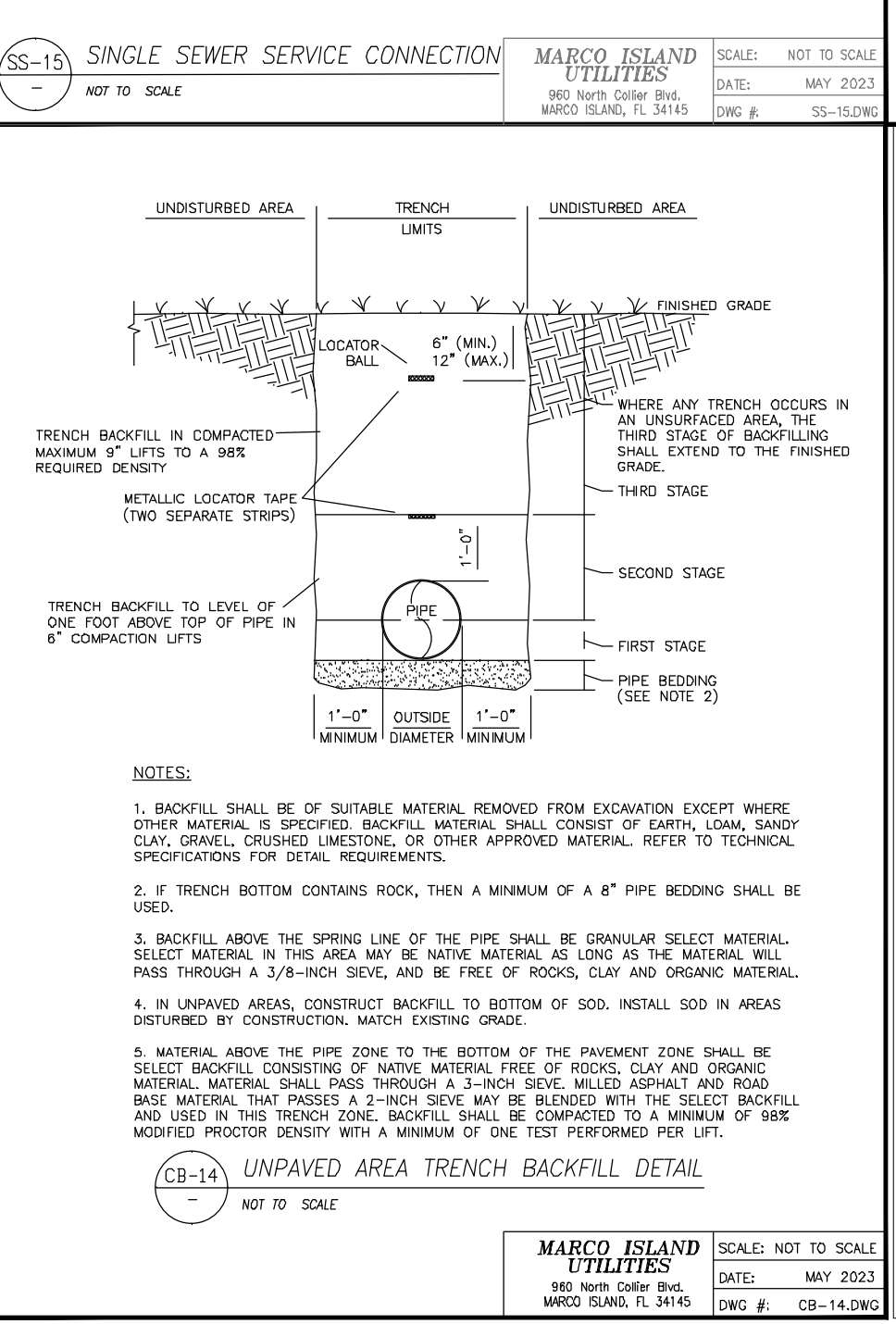
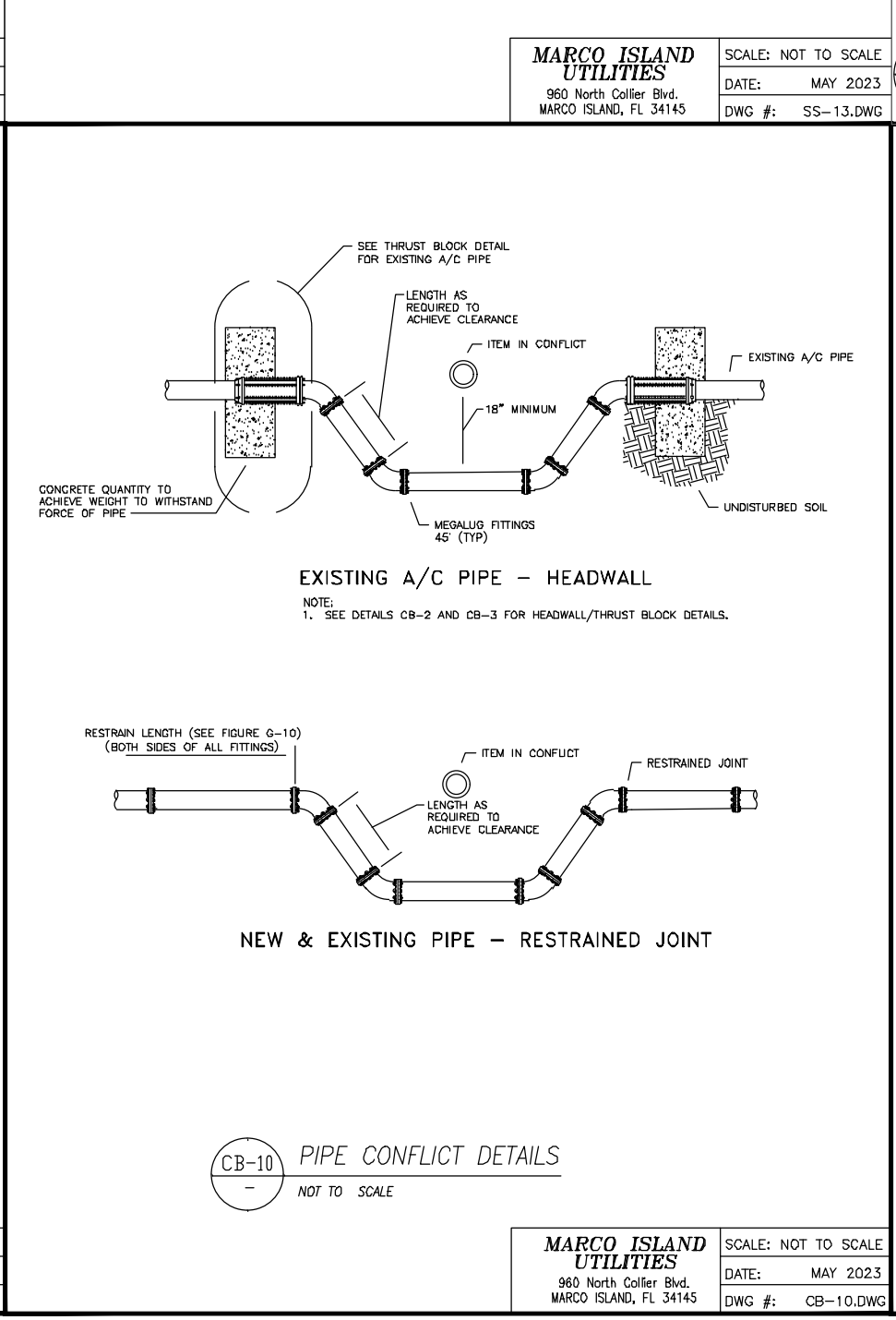
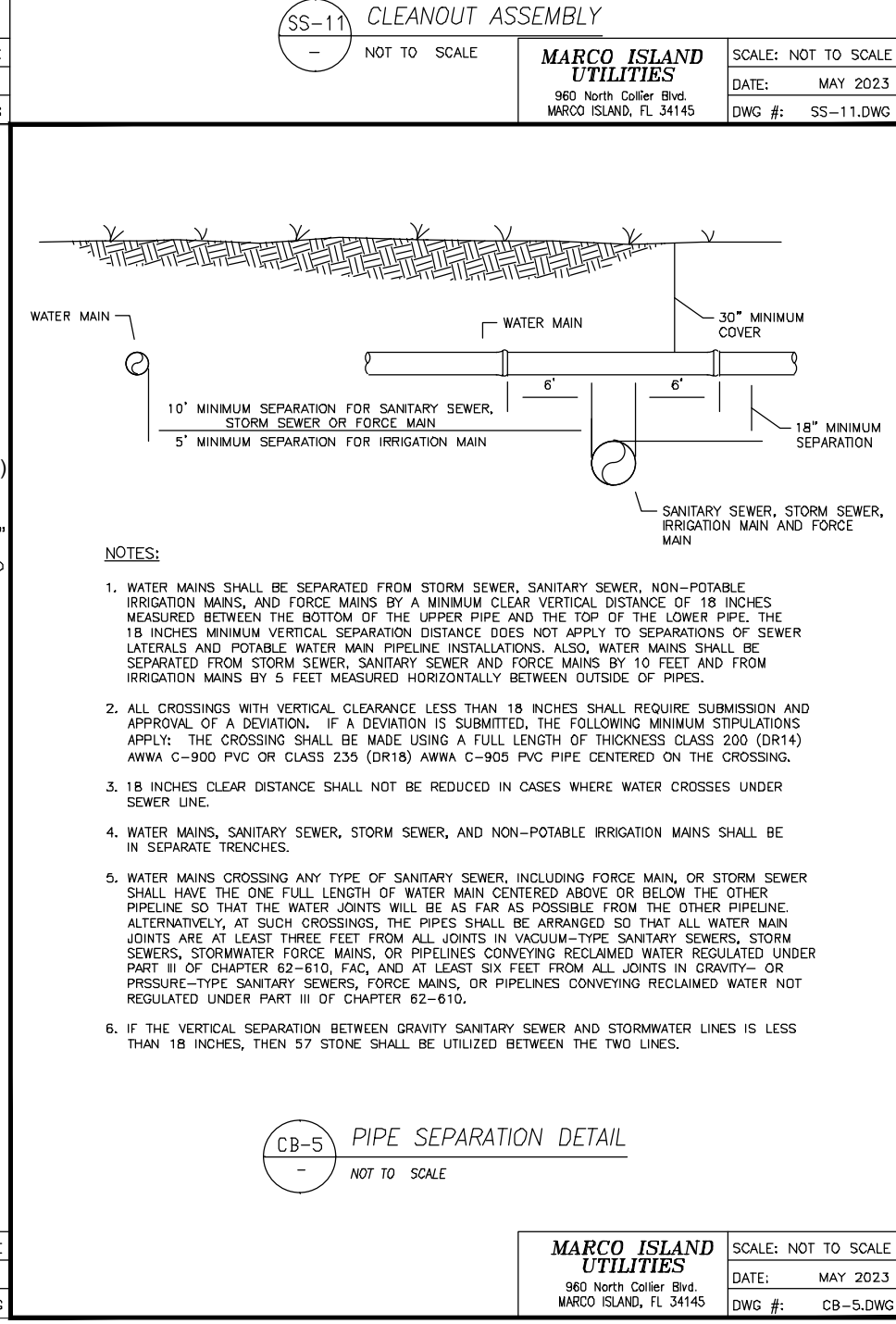
SINGLE SERVICE	CONNECTION TO MAIN	DOUBLE SERVICE	CONNECTION TO MAIN	BRANCH SIZE
2" METER	1-1/2"	(2) 2" METERS	1-1/2"	1"
1" METER	1-1/2"	(2) 1" METERS	1-1/2"	1"
1-1/2" METER	1-1/2"			
2" METER	2"			

NOTES:
1. 1-1/2" AND LARGER METERS SHALL BE SERVED BY SINGLE SERVICE ONLY.
2. WE CONSIDER SEE APPROVED PRODUCT LIST SHALL BE USED FOR MULTI-SERVICE. SUCCESSIVE TAPS INTO WATER MAIN WILL BE NO LONGER THAN 24" APART.
3. ALL GROUND PIPES SHALL EXTEND A MINIMUM OF 6" BEYOND THE EDGE OF PAVEMENT, WITH A CAPPING CHAMFER TO BE NO LESS THAN 4". COVER SHALL BE FINISHED WITH AN ELECTRONIC MARKER (SEE COUNTY APPROVED PRODUCT LIST APPROVED TO).
4. TAPPING SCHEDULE, CONNECTION STOP, POLY TRENCH, CURB STOP, AND METAL RINGS SHALL BE INSTALLED BY CONTRACTOR AT THE TIME OF WATER MAIN INSTALLATION.
5. ALL PLUMBING SHALL BE A MINIMUM 3" FROM METER BOX, AND SHALL PROVIDE A 2" OFFSET.
6. ALL COMPONENTS THAT COME INTO CONTACT WITH DRINKING WATER SHALL CONFORM TO NSF STANDARD #1 AND THE REDUCTION OF LEAD IN DRINKING WATER NOT EXCEEDING THE SAFE DRINKING WATER ACT.
7. ALL CONDITIONS ARE FOR PRODUCT MODEL NUMBERS, SEE APPROVED PRODUCT LIST.



W-23 TYPICAL THRUST BLOCKS

PIPE DIA (IN)	BEARING AREA "A" (SQ FT)	VERTICAL THRUST BLOCK VOL. OF CONCRETE (CY)
8	1.9	1.0
10	3.3	1.0
12	5.4	1.1
14	7.6	1.3
16	10.3	1.5
18	13.4	1.8
20	17.2	2.2
24	28.8	3.6



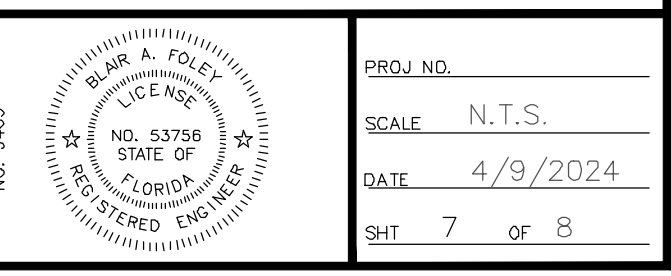
DESIGN: BAF
DRAWN: AOM
CHECKED: BAF
O.C.:

NO. DATE REVISION APPD BY

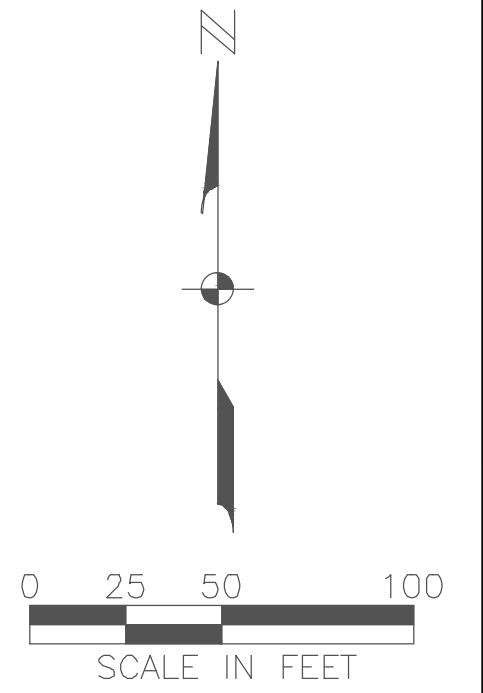
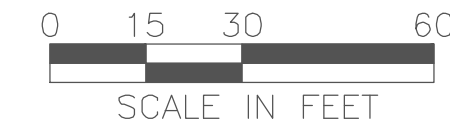
Blair A. Foley, P.E., LLC
Civil Engineer / Development Consultant
120 Edgewater Way South - Naples, FL 34105
Phone (239) 263-1222 Cell (239) 289-4900 Fax (239) 263-0472 E-mail fols000@aol.com

CLIENT: MARCO LUXE LLC
MARCO LUXE
Collier County, Florida

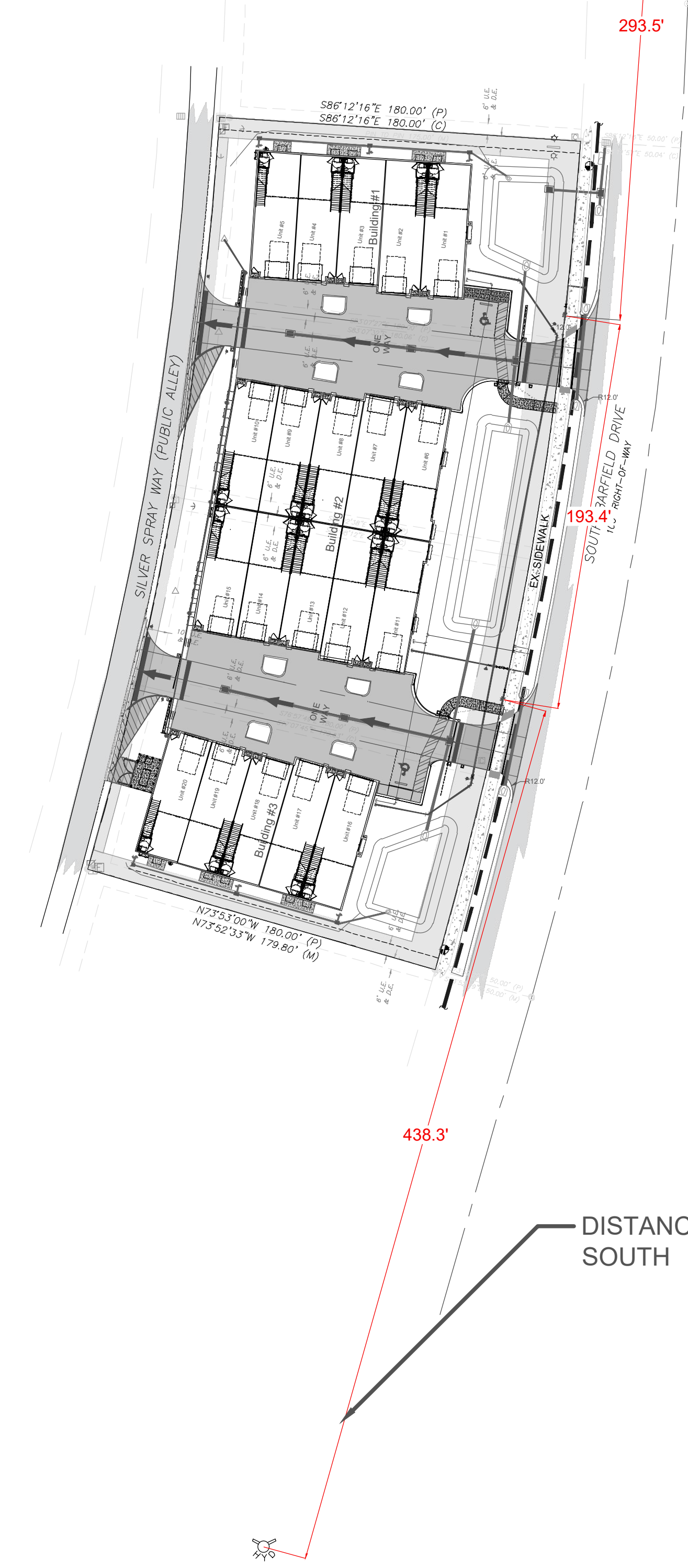
DETAILS
SCALE: N.T.S.
DATE: 4/9/2024
SHT 7 of 8



AUTOTURN SWEEP PATH ANALYSIS
FOR THE PIERCE QUANTUM 105'
AERIAL TRUCK
DRIVEWAY ADAPTION FOR LEFT TURN



DISTANCE TO NEAREST OFFSITE HYDRANT
NORTH



DISTANCE TO NEAREST OFFSITE HYDRANT
SOUTH

NOTE: FIRE HYDRANT MUST BE CLOW MEDALLION

FIRE METER: 6" NEPTUNE C&I MACH 10 R900, 2.0 TO 2,000 US GPM, FIRE FLOW 1,800 US GPM

FIRE FLOW REQUIREMENTS
FBC TYPE IIB, SPRINKLED

	FLOW REQUIREMENT	75% RED.	MIN. REQUIREMENT
Building 1: 6,909 SF	1,750 GPM 2 HR	438 GPM	1,000 GPM
Building 2: 13,991 SF	2,500 GPM 2 HR	625 GPM	1,000 GPM
Building 3: 6,909 SF	1,750 GPM 2 HR	438 GPM	1,000 GPM

According to NFPA Table 18.4.5.2.1 Fire Flow Requirement,
75% Reduction Allowed for Sprinkled Buildings OR 1000 GPM Min = **1,000 GPM**

LEGEND

- PROPOSED ASPHALT
- DRY DETENTION
- PROPOSED 4" CONC.
- EXISTING GRADE
- PROPOSED GRADE
- MITERED END SECTION
- CATCH BASIN

DESIGN	BAF	5				
DRAWN	AOM	4				
CHECKED	BAF	3				
		2				
		1	6/27/24	City of Marco Comment Letter dated 6/11/24	BAF	
G.C.		NO.	DATE	REVISION	APPD BY	

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CLIENT: MARCO LUXE LLC
MARCO LUXE
Collier County, Florida

FIRE PLAN

PROJ. NO. _____
SCALE: SEE BAR SCALE
DATE: 6/24/2024
SHT 8 OF 8